



Project Location

# SHELBY COUNTY

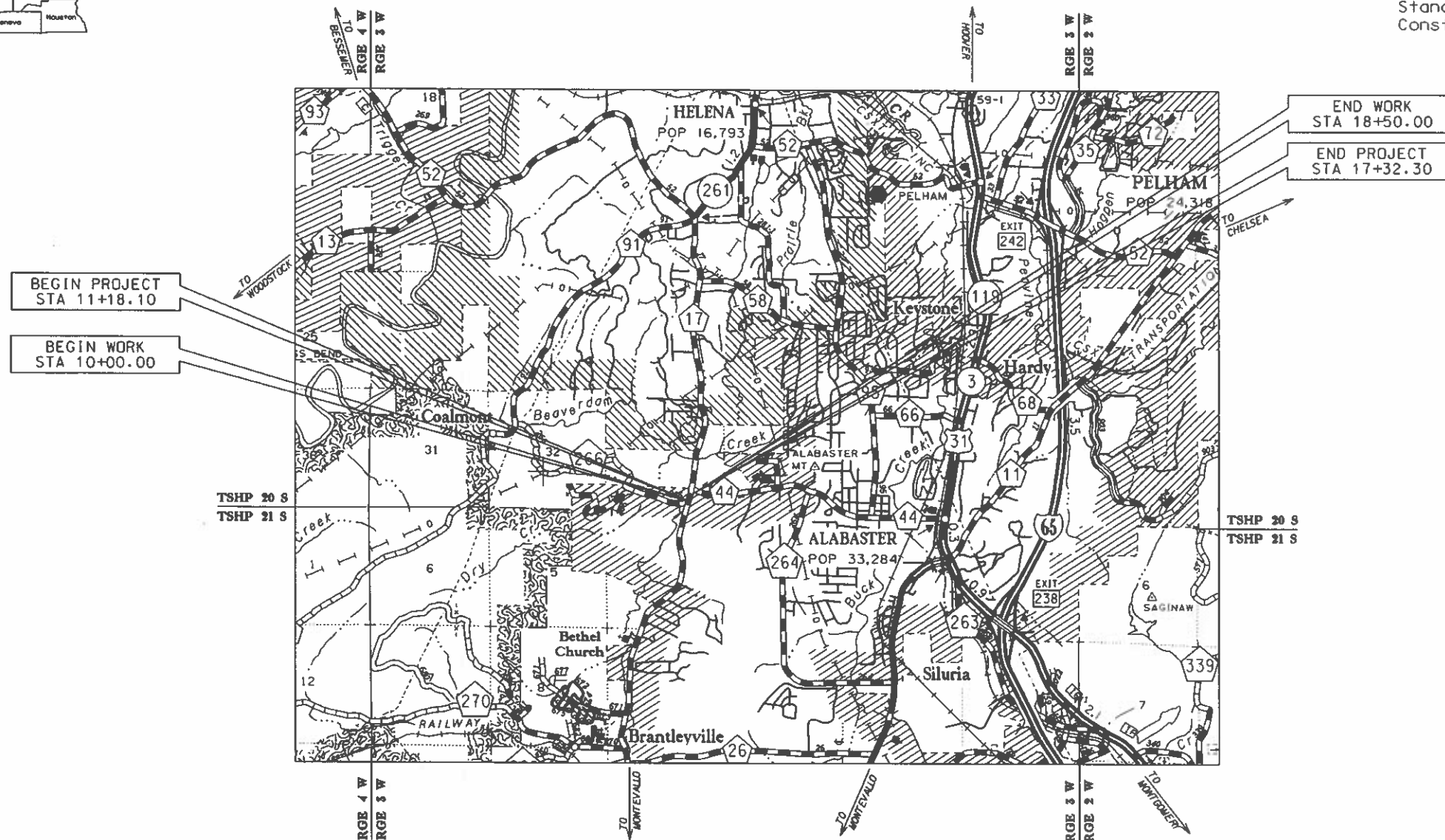
## PLANS OF PROPOSED PROJECT

### RIGHT TURN LANE ON COUNTY ROAD 17 AT COUNTY ROAD 44 GRADE, DRAIN, BASE AND PAVE, AND WIDENING SHELBY COUNTY

| STATE          | REFERENCE<br>PROJECT NO. | FISCAL<br>YEAR | SHEET<br>NO. | LAST<br>SHEET<br>NO. |
|----------------|--------------------------|----------------|--------------|----------------------|
| AL             | SCP 59-889-18            | 2023           | 1            | 60                   |
| CONTRACT ID NO |                          |                |              |                      |

| Design Designation               |      |
|----------------------------------|------|
| ADT ( 2018 ).....                | XX   |
| ADT ( 2038 ).....                | XX   |
| K .....                          | XX   |
| D .....                          | XX   |
| TDHV .....                       | XX   |
| TADT .....                       | XX   |
| V ( Design Speed ) (m.p.h.)..... | 45   |
| Min. Stopping Sight Dist.....    | 360' |

These plans have been prepared to conform to the Alabama Department of Transportation Standard Specifications for Highway Construction, 2022 Edition.



#### EQUATIONS AND EXCEPTIONS:

N/A

|                             |                    |
|-----------------------------|--------------------|
| Total Stationing of Project | 614.20 FT          |
| Equations & Exceptions      | N/A                |
| Net Length of Project       | 614.20 FT 0.116 MI |
| Net Length of Roadways      | 614.20 FT 0.116 MI |

#### VICINITY MAP

POPULATIONS ARE FROM 2020 CENSUS

SHELBY COUNTY HIGHWAY DEPARTMENT  
Submitted for Approval  
*James R. Brown*  
COUNTY ENGINEER

PLANS PREPARED BY:

**GSA**  
GONZALEZ - STRENGTH & ASSOCIATES, INC.  
ENGINEERING, LAND PLANNING, SURVEYING, TRAFFIC/TRANSPORTATION



*James R. Brown* 1-11-23  
JAMES R. BROWN, PE  
AL PROFESSIONAL REGISTRATION NO. 15858

| INDEX TO SHEETS |  |                                    |                             |                |  |              | REFERENCE<br>PROJECT NO | FISCAL<br>YEAR   | SHEET<br>NO |
|-----------------|--|------------------------------------|-----------------------------|----------------|--|--------------|-------------------------|------------------|-------------|
|                 |  |                                    |                             |                |  |              | SCP 59-889-18           | 2023             | 1-A         |
| SHEET NO        |  | DESCRIPTION                        |                             |                |  |              |                         |                  |             |
| 1               | TITLE SHEET  |                                    |                             |                |  |              |                         |                  |             |
| 1-A             | INDEX TO SHEETS  |                                    |                             |                |  |              |                         |                  |             |
| 1-B             | INDEX TO SPECIAL AND STANDARD DRAWINGS                             |                                    |                             |                |  |              |                         |                  |             |
| 1-C             | PLANS LEGEND SHEET   |                                    |                             |                |  |              |                         |                  |             |
| 1-D             | PLANS ABBREVIATIONS SHEET  |                                    |                             |                |  |              |                         |                  |             |
| 1-E             | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 1-F             | PRIMARY SURVEY CONTROL AND GEOMETRIC LAYOUT SHEET                  |                                    |                             |                |  |              |                         |                  |             |
| 1-G             | GEOMETRIC LAYOUT SHEET - HORIZONTAL ALIGNMENT DATA                 |                                    |                             |                |  |              |                         |                  |             |
| 2               | TYPICAL SECTION SHEET  |                                    |                             |                |  |              |                         |                  |             |
| 2-A             | PROJECT NOTE SHEET   |                                    |                             |                |  |              |                         |                  |             |
| 2-B             | GENERAL TRAFFIC CONTROL PLAN NOTES                                 |                                    |                             |                |  |              |                         |                  |             |
| 3 THRU 3-B      | SUMMARY OF QUANTITIES  |                                    |                             |                |  |              |                         |                  |             |
| 4               | PLAN SHEET   |                                    |                             |                |  |              |                         |                  |             |
| 4-A             | PROFILE SHEET  |                                    |                             |                |  |              |                         |                  |             |
| 5               | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 6               | PAVING LAYOUT, SIGNING AND STRIPING SHEET                          |                                    |                             |                |  |              |                         |                  |             |
| 7               | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 8               | UTILITY PLAN SHEET   |                                    |                             |                |  |              |                         |                  |             |
| 9               | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 10              | TRAFFIC CONTROL SEQUENCE OF CONSTRUCTION AND SUMMARY OF QUANTITIES |                                    |                             |                |  |              |                         |                  |             |
| 11              | TRAFFIC CONTROL PLAN   |                                    |                             |                |  |              |                         |                  |             |
| 12 THRU 14      | TRAFFIC CONTROL PLAN SPECIAL PROJECT DETAILS                       |                                    |                             |                |  |              |                         |                  |             |
| 15 THRU 29      | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 30              | EROSION AND SEDIMENT CONTROL LEGEND                                |                                    |                             |                |  |              |                         |                  |             |
| 31              | EROSION AND SEDIMENT CONTROL PLAN SHEET- INITIAL PHASE             |                                    |                             |                |  |              |                         |                  |             |
| 32              | EROSION AND SEDIMENT CONTROL PLAN SHEET- INTERMEDIATE PHASE        |                                    |                             |                |  |              |                         |                  |             |
| 33              | EROSION AND SEDIMENT CONTROL PLAN SHEET- FINAL CONSTRUCTION        |                                    |                             |                |  |              |                         |                  |             |
| 34 THRU 40      | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 41              | DRAINAGE CROSS SECTION SHEET                                       |                                    |                             |                |  |              |                         |                  |             |
| 42 THRU 49      | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 50 THRU 56      | ROADWAY CROSS SECTION SHEETS - CR-17                               |                                    |                             |                |  |              |                         |                  |             |
| 57 THRU 59      | OMIT   |                                    |                             |                |  |              |                         |                  |             |
| 60              | EARTHWORK SUMMARY SHEET  |                                    |                             |                |  |              |                         |                  |             |
|                 | RESPONSIBLE PE: James R. Brown, P.E.                               | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  | NOT TO SCALE | SHEET TITLE             | ROUTE            |             |
|                 | DATE:  | DATE:                              | DATE:                       | PERMIT         |  |              | INDEX TO SHEETS         | CR-17 @<br>CR-44 |             |

| SPECIAL PROJECT DETAIL   |                                       |                 |  |           |                              |                | REFERENCE PROJECT NO  | FISCAL YEAR | SHEET NO                        |                  |
|--|---------------------------------------|-----------------|--|-----------|------------------------------|----------------|---|-------------|---------------------------------|------------------|
|  |                                       |                 |  |           |                              |                | SCP 59-889-18   | 2023        | 1-B                             |                  |
| THE FOLLOWING ARE SPECIAL OR STANDARD DRAWINGS CONTAINED IN THE ALABAMA DEPARTMENT OF TRANSPORTATION SPECIAL & STANDARD HIGHWAY DRAWINGS BOOK (U.S. CUSTOMARY UNITS OF MEASUREMENTS) DATED 2022 WHICH APPLY TO THIS PROJECT: |                                       |                 |  |           |                              |                |   |             |                                 |                  |
| INDEX NO.  | DRAWING NO.                           |                 | DESCRIPTION  | INDEX NO. | DRAWING NO.                  |                | DESCRIPTION   |             |                                 |                  |
| 53004  | RPC-530                               | (SHEET 1 OF 3)  | BEDDING AND FILL HEIGHTS FOR ALL ROADWAY PIPE CULVERTS (RCP AND CMP)   | 71050     | SHS-0                        | (SHEET 1 OF 4) | STANDARD HIGHWAY SIGNS INDEX  |             |                                 |                  |
| 53005  | RPC-530                               | (SHEET 2 OF 3)  | BEDDING AND FILL HEIGHTS FOR ALL ROADWAY PIPE CULVERTS (CMP AND RCP)   | 71051     | SHS-0                        | (SHEET 2 OF 4) | STANDARD HIGHWAY SIGNS INDEX  |             |                                 |                  |
| 53006  | RPC-530                               | (SHEET 3 OF 3)  | BEDDING AND FILL HEIGHTS FOR ALL ROADWAY PIPE CULVERTS (H.D.P.E. PIPE)   | 71052     | SHS-0                        | (SHEET 3 OF 4) | STANDARD HIGHWAY SIGNS INDEX  |             |                                 |                  |
| 60201  | M-602                                 |                 | DETAILS OF MONUMENTS TO BE USED FOR REFERENCE OF CARDINAL POINTS OF HIGHWAY R.O.W. LINE AND LAND SURVEY CORNERS                      | 71053     | SHS-0                        | (SHEET 4 OF 4) | STANDARD HIGHWAY SIGNS INDEX  |             |                                 |                  |
| 61905  | HW-614-B (PC)                         | (SHEET 1 OF 2)  | PRECAST SLOPE PAVED HEADWALL DETAILS FOR RCP AND CMP ROADWAY PIPE  | 71060     | SHS-1                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 61906  | HW-614-B (PC)                         | (SHEET 2 OF 2)  | PRECAST SLOPE PAVED HEADWALL DETAILS FOR RCP AND CMP ROADWAY PIPE  | 71061     | SHS-2                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 61909  | HW-614-B                              | (SHEET 1 OF 2)  | SLOPE PAVED HEADWALL DETAILS FOR REINFORCED CONCRETE AND CORRUGATED METAL ROADWAY PIPE   | 71062     | SHS-3                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 61910  | HW-614-B                              | (SHEET 2 OF 2)  | SLOPE PAVED HEADWALL DETAILS FOR REINFORCED CONCRETE AND CORRUGATED METAL ROADWAY PIPE   | 71063     | SHS-4                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 62001  | CC-530                                |                 | DETAILS OF CONCRETE COLLAR FOR CONNECTING CONCRETE PIPE OF DIFFERENT SHELL THICKNESS OR CONNECTING CONCRETE TO CORRUGATED METAL PIPE | 71064     | SHS-5                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 65901  | ESC-509                               |                 | DETAILS OF ROLLED AND HYDRAULIC EROSION CONTROL PRODUCT INSTALLATION   | 71065     | SHS-6                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66501  | ESC-100-1                             |                 | BEST MANAGEMENT PRACTICE REFERENCE MATRIX  | 71066     | SHS-7                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66502  | ESC-100-2                             |                 | BEST MANAGEMENT PRACTICE REFERENCE MATRIX  | 71067     | SHS-8                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66505  | ESC-200-1                             |                 | TYPICAL TEMPORARY EROSION / SEDIMENT CONTROL APPLICATIONS  | 71068     | SHS-9                        |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66506  | ESC-200-2                             |                 | DETAILS OF TEMPORARY SLOPE DRAIN, BERMS, AND ENERGY DISSIPATOR   | 71069     | SHS-10                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66507  | ESC-200-3                             |                 | DETAILS OF SEDIMENT BARRIER APPLICATIONS   | 71072     | SHS-11                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66508  | ESC-200-4                             |                 | DETAILS OF SILT FENCE INSTALLATION   | 71074     | SHS-12                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66509  | ESC-200-5                             |                 | DETAILS OF SEDIMENT RETENTION BARRIER  | 71075     | SHS-13                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66512  | ESC-300-1                             |                 | DITCH CHECK STRUCTURES, TYPICAL APPLICATIONS AND DETAILS   | 71076     | SHS-14                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66513  | ESC-300-2                             |                 | DETAILS OF HAY BALE DITCH CHECKS   | 71077     | SHS-15                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66514  | ESC-300-3                             |                 | DETAILS OF SANDBAG DITCH CHECK   | 71078     | SHS-16                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66515  | ESC-300-4                             |                 | DETAILS OF EROSION CONTROL WATTLE DITCH CHECKS   | 71079     | SHS-17                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66516  | ESC-300-5                             |                 | DETAILS OF SILT DIKE DITCH CHECKS  | 71080     | SHS-18                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66517  | ESC-300-6                             |                 | DETAILS OF ROCK DITCH CHECKS   | 71082     | SHS-19                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66518  | ESC-300-7                             |                 | DETAILS OF ROCK DITCH CHECKS WITH SUMP EXCAVATION  | 71083     | SHS-20                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66519  | ESC-300-8                             |                 | DETAILS OF SILT FENCE DITCH CHECKS   | 71084     | SHS-21                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66522  | ESC-400-1                             |                 | INLET PROTECTION TYPICAL APPLICATIONS AND DETAILS  | 71085     | SHS-22                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66523  | ESC-400-2                             |                 | INLET PROTECTION DETAILS FOR COARSE AGGREGATE ON GRADES AND SAGS   | 71086     | SHS-23                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66524  | ESC-400-3                             |                 | INLET PROTECTION DETAILS OF WATTLES  | 71087     | SHS-24                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66525  | ESC-400-4                             |                 | INLET PROTECTION DETAILS OF SILT FENCE   | 71088     | SHS-25                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66526  | ESC-400-5                             |                 | INLET PROTECTION DETAILS OF SAND BAGS  | 71090     | SHS-26                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66529  | ESC-501                               |                 | FLOATING BASIN BOOM  | 71091     | SHS-27                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66532  | ESC-502                               |                 | STABILIZED CONSTRUCTION ENTRANCE   | 71092     | SHS-28                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66535  | ESC-503                               |                 | TEMPORARY DEWATERING STRUCTURES  | 71093     | SHS-29                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66538  | ESC-504                               |                 | TEMPORARY CULVERT STREAM CROSSING  | 71094     | SHS-30                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66541  | ESC-505                               |                 | TEMPORARY STREAM DIVERSION   | 71095     | SHS-31                       |                | STANDARD HIGHWAY SIGNS  |             |                                 |                  |
| 66544  | ESC-506-1                             |                 | SUSPENDED PIPE DIVERSION (DOWNSTREAM)  | 74001     | B-107-2                      |                | PERFORATED SQUARE STEEL TUBING (PSST) BARRICADES TYPE I, TYPE II, AND TYPE III & VERTICAL PANELS TYPE I AND TYPE II |             |                                 |                  |
| 66545  | ESC-506-2                             |                 | SUSPENDED PIPE DIVERSION (UPSTREAM)  | 74007     | TCD-100                      |                | DETAILS FOR TRAFFIC CHANNELIZATION DEVICES  |             |                                 |                  |
| 66548  | ESC-507                               |                 | TEMPORARY SEDIMENTATION BASIN  | 74201     | PCMS-710                     | (SHEET 1 OF 3) | DETAILS OF PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)   |             |                                 |                  |
| 67201  | ESC-508                               |                 | FLOCCULANT USAGE GUIDE   | 74202     | PCMS-710                     | (SHEET 2 OF 3) | DETAILS OF PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)   |             |                                 |                  |
| 68001  | GN-2 NOTES                            |                 | STANDARD DESIGN NOTES FOR PLAN ASSEMBLIES  | 74203     | PCMS-710                     | (SHEET 3 OF 3) | DETAILS OF PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)   |             |                                 |                  |
| 68004  | TO-107                                |                 | DETAILS OF INTERSECTIONS AND TURNOUTS  |           |                              |                |   |             |                                 |                  |
| 68013  | LT-703                                |                 | STANDARD TRANSITION TAPERS FOR 2, 3, AND 4 LANE HIGHWAYS   |           |                              |                |   |             |                                 |                  |
| 68016  | SSEC-1                                | (SHEET 1 OF 14) | STANDARD SUPERELEVATION OF CURVES  |           |                              |                |   |             |                                 |                  |
| 68017  | SSEC-1                                | (SHEET 2 OF 14) | STANDARD SUPERELEVATION OF CURVES  |           |                              |                |   |             |                                 |                  |
| 70101  | PS-701-6                              |                 | DETAILS OF TRAFFIC STRIPING FOR 2 LANE HIGHWAYS  |           |                              |                |   |             |                                 |                  |
| 70133  | PS-701-7                              |                 | STRIPING DETAILS FOR DROP LANES AND TURN LANES   |           |                              |                |   |             |                                 |                  |
| 70301  | TCM-703                               | (SHEET 1 OF 2)  | PAVEMENT LEGENDS AND MARKINGS  |           |                              |                |   |             |                                 |                  |
| 70302  | TCM-703                               | (SHEET 2 OF 2)  | PAVEMENT LEGENDS AND MARKINGS  |           |                              |                |   |             |                                 |                  |
| 70501  | PM-705-1                              |                 | DETAILS OF PAVEMENT MARKERS CLASS A, A-H, AND B  |           |                              |                |   |             |                                 |                  |
| 70504  | PM-705-2                              |                 | DETAILS SHOWING APPLICATION OF PAVEMENT MARKERS  |           |                              |                |   |             |                                 |                  |
| 70507  | PM-705-3                              | (SHEET 1 OF 3)  | DETAILS OF RAISED PAVEMENT MARKERS, MARKINGS, AND STRIPE AT RAMPS, RAMP GORES, AND MAINLINE  |           |                              |                |   |             |                                 |                  |
| 70508  | PM-705-3                              | (SHEET 2 OF 3)  | DETAILS OF RAISED PAVEMENT MARKERS, MARKINGS, AND STRIPE AT RAMPS, RAMP GORES, AND MAINLINE  |           |                              |                |   |             |                                 |                  |
| 70509  | PM-705-3                              | (SHEET 3 OF 3)  | DETAILS OF RAISED PAVEMENT MARKERS, MARKINGS, AND STRIPE AT RAMPS, RAMP GORES, AND MAINLINE  |           |                              |                |   |             |                                 |                  |
| 70901  | MP-710                                |                 | DETAILS OF MILEPOSTS FOR 2 LANE AND 4 LANE HIGHWAYS USING ONE, TWO, OR THREE   |           |                              |                |   |             |                                 |                  |
| 71001  | IHS-710-1                             | (SHEET 1 OF 2)  | WIND VELOCITY CHART FOR ROADSIDE SIGNS   |           |                              |                |   |             |                                 |                  |
| 71002  | IHS-710-1                             | (SHEET 2 OF 2)  | DESIGN CHARTS FOR BEAM SIGN SUPPORTS AND FOOTINGS  |           |                              |                |   |             |                                 |                  |
| 71017  | IHS-710-12                            |                 | DETAILS OF ROADWAY SIGN POST (SMALL CHANNEL AND TUBULAR SECTION)   |           |                              |                |   |             |                                 |                  |
| 71032  | IHS-710-21                            |                 | DETAILS FOR LOCATION AND MOUNTING STANDARD FLAT PANEL SIGNS ON U-CHANNEL AND TUBULAR POSTS   |           |                              |                |   |             |                                 |                  |
| 71035  | IHS-710-23                            |                 | LIGHTWEIGHT STRUCTURAL SIGN SUPPORT INSTALLATIONS  |           |                              |                |   |             |                                 |                  |
| 71041  | SL-710                                |                 | TYPICAL STOP AND YIELD SIGN LOCATIONS  |           |                              |                |   |             |                                 |                  |
|  | RESPONSIBLE PE: James R. Brown, P. E. |                 | SUPERVISOR: Doug R. Peterson, P. E.  |           | DESIGNER: Dayla Baugh, P. E. |                | PLAN SUBMITTAL  |             | NOT TO SCALE                    |                  |
|  | DATE:                                 |                 | DATE:  |           | DATE:                        |                | PERMIT  |             |                                 |                  |
|  |                                       |                 |  |           |                              |                |   |             | SHEET TITLE                     | ROUTE            |
|  |                                       |                 |  |           |                              |                |   |             | SPECIAL PROJECT<br>DETAIL SHEET | CR-17 @<br>CR-44 |

| PLANS LEGEND  |  |  |  |  |  | REFERENCE<br>PROJECT NO  | FISCAL<br>YEAR | SHEET<br>NO |
|---|--|--|--|--|--|--|----------------|-------------|
|   |  |  |  |  |  | SCP 59-889-18  | 2023           | 1-C         |
| <div><div>ROADWAY</div><div><div><div>CENTER LINE</div><div>STATE BOUNDARY LINE</div><div>COUNTY BOUNDARY LINE</div><div>CITY OR TOWN LIMITS</div><div>SECTION LINES</div><div>QUARTER-SECTION LINES</div><div>RANGE-TOWNSHIP LINES</div><div>PROPERTY LINES</div><div>PRESENT ROW</div><div>ACQUIRED ROW</div><div>DENIED ACCESS</div><div>REQUIRED FENCE</div><div>CONSTRUCTION LIMITS</div><div>CLEARING LIMITS</div><div>RAILROAD</div><div>EXISTING WOOD FENCE</div><div>EXISTING BARBED WIRE FENCE</div><div>EXISTING CHAIN LINK FENCE</div><div>EXISTING ELECTRIC FENCE</div><div>EXISTING HOG WIRE FENCE</div><div>TREES</div><div>WOODS LINE</div><div>WETLANDS</div><div>EXISTING DITCH</div><div>REQUIRED DITCH</div><div>GRAVEL ROAD</div><div>EXISTING GUARDRAIL</div><div>REQUIRED GUARDRAIL</div><div>SATELLITE DISH</div><div>TRAFFIC LIGHT</div><div>BENCH MARK</div><div>SURVEY POINT</div><div>ENVIRONMENTAL CLEARED LIMITS</div></div><div><div>55+00</div><div>SEC 22</div><div>SEC 23</div><div>R-17-E</div><div>R-18-E</div><div>CONST LIM</div><div>EL 410.25</div><div>EL 420.55</div></div><div><div>EXISTING PIPE</div><div>REQUIRED PIPE</div><div>(WITH PIPE END TREATMENT)</div><div>REQUIRED PIPE END TREATMENT</div><div>EXISTING BOX CULVERT</div><div>REQUIRED BOX CULVERT</div><div>EXTENDED CULVERT</div><div>DROP INLET OR JUNCTION BOX</div><div>(SEE PLANS DESCRIPTION )</div><div>BRIDGE</div><div>PIPE CULVERT (ELEVATION VIEW)</div><div>BOX CULVERT (ELEVATION VIEW)</div></div><div><div>DRAINAGE STRUCTURE INDEX NUMBERS</div><div>DRAINAGE STRUCTURE WRITE-UPS ARE LOCATED ON THE DRAINAGE CROSS-SECTION SHEETS. STRUCTURES WITH WRITE-UPS ARE INDEXED AT EACH END, WITH NUMBERS ASSIGNED BY DIRECTION OF FLOW. THE NUMBER IN THE UPPER HALF OF THE CIRCLE (EXAMPLE 8 OR 9) IS THE DRAINAGE STRUCTURE INDEX NUMBER. THE NUMBER IN THE LOWER HALF (EXAMPLE 88) IS THE SHEET REFERENCE NUMBER.</div><div><div>8</div><div>88</div><div>9</div><div>88</div><div>REQD 18" RCP</div><div>ROADWAY PIPE EXAMPLE</div><div>ALL INFORMATION CONCERNING THE DISPOSITION OF SIDE DRAIN PIPE IS SHOWN ON THE SUMMARY OF QUANTITIES BOX SHEET. THE TOP LETTERS (SD) ARE FOR SIDE DRAIN AND THE BOTTOM NUMBER IS THE DRAINAGE STRUCTURE INDEX NUMBER.</div><div><div>SD</div><div>4</div><div>REQD 18"</div><div>SIDE DRAIN PIPE EXAMPLE</div><div>DIRECTION OF FLOW</div></div></div></div></div></div> |  |  |  |  |  | <div><div>UTILITIES</div><div><div>EXISTING</div><div>PROPOSED</div></div><div><div>POWER POLE</div><div>LIGHT POLE</div><div>TELEPHONE POLE</div><div>ANCHOR</div><div>STUB (POWER)</div><div>STUB (TELEPHONE)</div><div>ELECTRIC DUCT</div><div>BURIED ELECTRIC CABLE</div><div>OVERHEAD ELECTRIC CABLE</div><div>ELECTRIC MANHOLE</div><div>TOWER</div><div>TELEPHONE PEDESTAL</div><div>TELEPHONE DUCT</div><div>BURIED TELEPHONE CABLE</div><div>OVERHEAD TELEPHONE CABLE</div><div>TELEPHONE MANHOLE</div><div>SANITARY SEWER</div><div>WATER LINE</div><div>WATER MAIN</div><div>WATER VALVE</div><div>FIRE HYDRANT</div><div>WATER METER</div><div>GAS LINE</div><div>GAS MAIN</div><div>GAS VALVE</div><div>GAS REGULATOR</div><div>BURIED CABLE TELEVISION</div><div>OVERHEAD CABLE TELEVISION</div></div><div><div>EL 4MTD</div><div>BE</div><div>OE</div><div>EL 4MTD</div><div>BE</div><div>OE</div><div>TP 4MTD</div><div>BTC</div><div>OTC</div><div>TMH</div><div>SS</div><div>W</div><div>WM</div><div>G</div><div>GM</div><div>BTV</div><div>OTV</div></div></div> |                |             |
| <div><div>EXISTING PAVEMENT (RETAIN)</div><div>EXISTING PAVEMENT (REMOVE)</div><div>EXISTING PAVEMENT (RETAIN AND OVERLAY)</div><div>CONCRETE (EXISTING OR REQUIRED)</div><div>EXISTING CONCRETE (REMOVE)</div><div>RIP RAP (EXISTING OR REQUIRED)</div></div>  |  |  |  |  |  | <div><div>SHEET TITLE</div><div>ROUTE</div></div> <div><div>PLANS LEGEND</div><div>CR-17 @ CR-44</div></div>   |                |             |
|   |  |  |  |  |  | NOT TO SCALE   |                |             |
| <div><div>RESPONSIBLE PE: James R. Brown, P.E.</div><div>SUPERVISOR: Doug R. Peterson, P.E.</div><div>DESIGNER: Dayla Baugh, P.E.</div><div>PLAN SUBMITTAL</div></div>  |  |  |  |  |  |  |                |             |
| <div><div>DATE:</div><div>DATE:</div><div>DATE:</div><div>PERMIT</div></div>  |  |  |  |  |  |  |                |             |

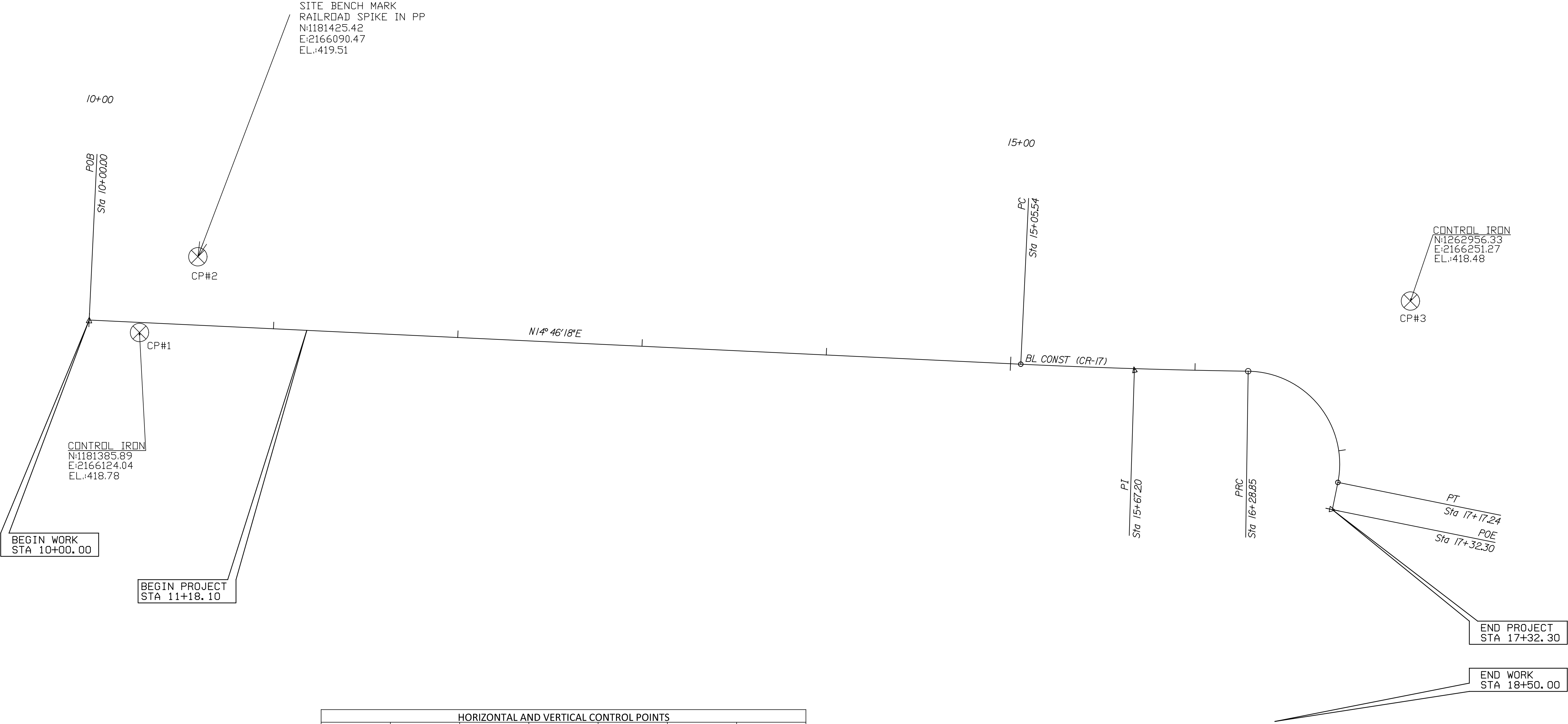
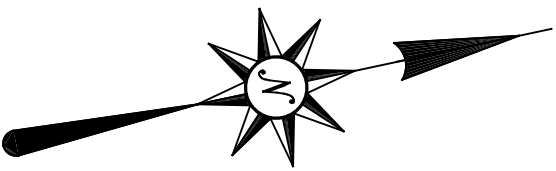


| PLANS ABBREVIATIONS SHEET                    |              |                                 |            |   | REFERENCE<br>PROJECT NO | FISCAL<br>YEAR                              | SHEET<br>NO |
|--|--------------|---------------------------------|------------|---|-------------------------|---|-------------|
|  |              |                                 |            |   | SCP 59-889-18           | 2023  | 1-D         |
|  |              |                                 |            |   |                         |   |             |
| ABANDON(ED).....                             | ABAN         | FLAT BOTTOM.....                | FB         | PROPOSED.....                             | PROP                    | WITH.....                                   | W/          |
| ABUTMENT.....                                | ABUT         | FLOW LINE.....                  | FL or FL   | QUADRUPLE.....                            | QUAD                    | WITNESS CORNER.....                         | WC          |
| ACCELERATION.....                            | ACCL         | FORESIGHT OR FRONTSIGHT.....    | FST or FST | QUADRUPLE BARREL CULVERT.....             | CQ                      | WOOD.....                                   | WD          |
| ACQUIRED.....                                | ACQD         | FRACTIONAL.....                 | FRAC       | QUANTITY.....                             | QUANT                   | WORKING POINT.....                          | WP          |
| ACRE.....                                    | AC           | FULL SUPERELEVATION.....        | FS         | RADIUS.....                               | R                       | WOVEN WIRE.....                             | W/W         |
| AHEAD.....                                   | AH           | GALLON.....                     | GAL        | RAILROAD.....                             | RR                      | YARD.....                                   | YD          |
| ALABAMA.....                                 | AL           | GASOLINE PUMPS.....             | GPP        | RANGE.....                                | RGE                     |   |             |
| ALABAMA DEPARTMENT OF TRANSPORTATION.....    | ALDOT        | GARAGE.....                     | GAR        | RECORD.....                               | REC                     | STRUCTURES                                  |             |
| ALTERNATE.....                               | ALT          | GAUGE.....                      | GA         | REDUCTION.....                            | RED                     |   |             |
| APPROXIMATE(LY).....                         | APP          | GIRDER.....                     | GDR        | REFERENCE.....                            | REF                     | NUMBER OF STORIES.....                      | 1, 2, 3, 4  |
| AREA.....                                    | A            | GOVERNMENT.....                 | GOV        | REFERENCE POINT.....                      | RP                      | FRAME.....                                  | FR          |
| ASPHALT.....                                 | ASP          | GRASS.....                      | GRS        | REFERENCE POINT FOR POINT ON TANGENT..... | RPPOT                   | BUILDING.....                               | BLDG        |
| AVERAGE ANNUAL DAILY TRAFFIC.....            | AADT         | GRADE CHANGE.....               | GC         | REINFORCED.....                           | REINF                   | BLOCK.....                                  | BLK         |
| BACK.....                                    | BK           | GRADE POINT.....                | GP         | REINFORCED CONCRETE.....                  | RC                      | BRICK.....                                  | BR          |
| BACK OF GUARDRAIL.....                       | BK-GR        | GRADE ROD.....                  | GRD        | REINFORCED CONCRETE DECK GIRDER.....      | RCDG                    | STUCCO.....                                 | STU         |
| BACKSIGHT.....                               | BS           | GRAVEL.....                     | GRV        | REINFORCED CONCRETE PIPE.....             | RCP                     | METAL.....                                  | MET         |
| BARBED WIRE.....                             | B/W          | GUARDRAIL.....                  | GR         | REINFORCING STEEL.....                    | REINF STL               | RESIDENCE.....                              | RES         |
| BARREL.....                                  | BBL          | HEADWALL.....                   | HDWL       | RELOCATE.....                             | RELC                    | BUSINESS.....                               | BUS         |
| BARRIER.....                                 | BAR          | HECTARE.....                    | HA         | REMOVE.....                               | REM                     | WAREHOUSE.....                              | WHSE        |
| BASE LINE.....                               | BL or BL     | HIGH WATER MARK.....            | HWM        | REQUIRED.....                             | REQD                    | CHICKEN HOUSE.....                          | CH HSE      |
| BEARING.....                                 | BRNG         | HEIGHT.....                     | HT         | RETAIN(ING).....                          | RET                     | CHURCH.....                                 | CH          |
| BEGIN.....                                   | BEG          | HEIGHT OF INSTRUMENT.....       | HI         | REVERSE CROWN.....                        | RC                      | SCHOOL.....                                 | SCH         |
| BEGINNING OF PROJECT.....                    | BOP          | HIGH WATER.....                 | HW         | REVISION.....                             | REV                     | DOUBLE WIDE MOBILE HOME.....                | DW MH       |
| BETWEEN.....                                 | BTW          | HIGHWAY.....                    | HWY        | RIGHT.....                                | RT                      | MOBILE HOME.....                            | MH          |
| BENCH MARK.....                              | BM           | HOGWIRE.....                    | H/W        | RIGHT AHEAD.....                          | RA                      |   |             |
| BILLBOARD.....                               | BBD          | HORIZONTAL.....                 | HOR        | RIGHT BACK.....                           | RB                      | UTILITIES                                   |             |
| BITUMINOUS.....                              | BT           | HUB & TACK.....                 | H&T        | RIGHT OF WAY.....                         | ROW                     |   |             |
| BITUMINOUS COATED CORRUGATED METAL PIPE..... | BCCMP        | HYDRANT.....                    | HYD        | RIGHT OF WAY MARKER.....                  | ROWM                    | ANCHOR WIRE.....                            | AW          |
| BOLLARD.....                                 | BDD          | IMPACT ATTENUATOR.....          | IA         | RIVER.....                                | RIV                     | BURIED ELECTRIC.....                        | BE          |
| BOUNDARY.....                                | BDY          | IN ACCORDANCE WITH.....         | I/A/W      | ROAD.....                                 | RD                      | BURIED FIBER OPTIC.....                     | BFO         |
| BRIDGE.....                                  | BRG          | IN PLACE.....                   | IN-PL      | ROADWAY.....                              | RDWY                    | BURIED TELEPHONE CABLE.....                 | BTC         |
| BRIDGE END SLAB.....                         | BES          | INCHES.....                     | IN         | SECTION.....                              | IN                      | BURIED CABLE TELEVISION.....                | BTV         |
| BRIDGE IDENTIFICATION NUMBER.....            | BIN          | INCLUDING.....                  | INCL       | SERVICE ROAD.....                         | SER RD                  | CAST IRON.....                              | CI          |
| CAPACITY.....                                | CAPY         | INCORPORATED.....               | INC        | SHEET.....                                | SHT                     | CIRCUIT.....                                | CKT         |
| CAST IRON.....                               | CT           | INSTRUMENT.....                 | INST       | SHEET PILING.....                         | SHT PILE                | DUCTILE IRON.....                           | DUC IRON    |
| CAST IN PLACE.....                           | CIP          | ISLAND.....                     | ISL        | SHOULDER.....                             | SHLD                    | EASEMENT.....                               | ESMT        |
| CATCH BASIN.....                             | CB           | JOINT.....                      | JT         | SIDE DRAIN.....                           | SD                      | FIBER OPTIC.....                            | FO          |
| CENTER LINE.....                             | CL or CL     | JUNCTION.....                   | JCT        | SIDEWALK.....                             | SW                      | FIRE HYDRANT.....                           | FH          |
| CHAIN LINK.....                              | C/L          | JUNCTION BOX.....               | JB         | SIGHT DISTANCE.....                       | S DIST                  | FORCED MAIN (SANITARY SEWER).....           | FM          |
| CLASS.....                                   | CLS          | KILOMETER.....                  | KM         | SINGLE BARREL CULVERT.....                | CS                      | GAS MAIN.....                               | GM          |
| CONCRETE.....                                | CONC         | KILOMETER POST.....             | KMP        | SKEW.....                                 | SK                      | GAS METER.....                              | GMET        |
| CONNECTION.....                              | CONN         | KILOMETERS PER HOUR.....        | KPH        | SLOPE STAKE.....                          | SST                     | GAS VALVE.....                              | GV          |
| CONSTRUCTION.....                            | CONST        | LANE.....                       | LN         | SOLID SODDING.....                        | SOL SOD                 | GUY WIRE.....                               | GUY         |
| CONSTRUCTION LIMITS.....                     | CONST LIM    | LATITUDE.....                   | LAT        | SOUTH.....                                | S                       | HIGH PRESSURE.....                          | HP          |
| CORNER.....                                  | COR          | LEFT.....                       | LT         | SOUTH BOUND.....                          | SB                      | KILOVOLT AMPS.....                          | KVA         |
| CORRECTION.....                              | CORR         | LEFT AHEAD.....                 | LA         | SOUTH BOUND ROADWAY.....                  | SBR                     | MANHOLE.....                                | MH          |
| CORRUGATED IRON.....                         | CORI         | LEFT BACK.....                  | LB         | SPECIAL.....                              | SP                      | MERCURY VAPOR LIGHT.....                    | MVL         |
| CORRUGATED METAL.....                        | CM           | LENGTH OF CURVE.....            | L          | SPECIAL DITCH.....                        | SP-DT                   | OVERHEAD FIBER OPTIC.....                   | OFO         |
| CORRUGATED METAL PIPE.....                   | CMP          | LINK.....                       | LK         | SPECIAL DITCH LEFT.....                   | SDL                     | OVERHEAD TELEPHONE CABLE.....               | OTC         |
| CORRUGATED PLASTIC PIPE.....                 | CPP          | LIMIT.....                      | LIM        | SPECIAL DITCH MEDIAN.....                 | SDM                     | OVERHEAD ELECTRIC CABLE.....                | OE          |
| COUNTY.....                                  | CO           | LINEAR.....                     | LIN        | SPECIAL DITCH RIGHT.....                  | SDR                     | OVERHEAD CABLE TELEVISION.....              | OTV         |
| COUNTY ROAD.....                             | CO-RD        | LINEAR FEET.....                | LIN FT     | SPECIAL DRAWING.....                      | SP-DWG                  | PAIR.....                                   | PR          |
| CREEK.....                                   | CK           | LONGITUDE.....                  | LONG       | SPECIFICATIONS.....                       | SPEC                    | PEDESTAL.....                               | PED         |
| CROSS SECTION.....                           | X-SECT       | MANHOLE.....                    | MH         | SPRING LINE.....                          | SL                      | POLY-VINYL CHLORIDE PIPE.....               | PVC         |
| CROWN REMOVED.....                           | CR           | MARKER.....                     | MRK        | SPIRAL TO CURVE.....                      | SC                      | POWER POLE.....                             | PP          |
| CUBIC FEET.....                              | FT3 or CU FT | MAXIMUM.....                    | MAX        | SPIRAL POINT OF INTERSECTION.....         | SPI                     | SANITARY SEWER.....                         | SS          |
| CUBIC FEET PER SECOND.....                   | CFS          | MEAN HIGH WATER.....            | MHW        | SPIRAL TO TANGENT.....                    | ST                      | SERVICE.....                                | SERV        |
| CUBIC YARD.....                              | YD3 or CU YD | MEAN LOW WATER.....             | MLW        | SQUARE.....                               | SQ                      | STEEL.....                                  | STL         |
| CUBIC METERS.....                            | M3           | MEASUREMENT.....                | MEAS       | SQUARE FEET.....                          | FT2 or SQ FT            | STORM DRAIN.....                            | STM         |
| CULVERT.....                                 | CULV         | MEDIAN.....                     | MED        | SQUARE METERS.....                        | M2                      | STORM SEWER.....                            | STMS        |
| CULTIVATED.....                              | CULT         | METER.....                      | M          | SQUARE YARD.....                          | YD2 or SQ YD            | SWITCH.....                                 | SW          |
| CURB FACE.....                               | CF           | MERIDIAN.....                   | MER        | STAKE.....                                | STK                     | TELEPHONE.....                              | TEL         |
| CURB AND GUTTER.....                         | C&G          | MILE POST.....                  | MP         | STANDARD.....                             | STD                     | TELEPHONE MANHOLE.....                      | TMH         |
| CUT.....                                     | C            | MILES.....                      | MI         | STANDARD DRAWING.....                     | STD-DWG                 | TRANSFORMER.....                            | TRAN        |
| CURVE TO SPIRAL.....                         | CS           | MILES PER HOUR.....             | MPH        | STANDARD STRENGTH.....                    | STD STR                 | TRANSMISSION LINE.....                      | TR LN       |
| DECELERATION.....                            | DECEL        | MILLIMETER.....                 | MM         | STATION.....                              | STA                     | TRIAXIAL CABLE (SERVICE).....               | TRIX        |
| DECLINATION.....                             | DECL         | MINIMUM.....                    | MIN        | STATION & ELEVATION.....                  | S/E                     | VITRIFIED CLAY PIPE.....                    | VCP         |
| DEGREE OF CURVE.....                         | D            | MONUMENT.....                   | MON        | STATION & OFFSET.....                     | SO                      | WATER MAIN.....                             | WM          |
| DENIED ACCESS.....                           | D/A          | MULTIPLE.....                   | MULT       | STOPPING SIGHT DISTANCE.....              | SSD                     | WATER METER.....                            | WMET        |
| DEPARTURE.....                               | DEP          | NORMAL.....                     | NORM       | STREET.....                               | ST                      | WATER VALVE.....                            | WV          |
| DESIGN SPEED.....                            | V            | NORMAL CROWN.....               | NC         | STRUCTURE.....                            | STR                     |   |             |
| DETACHABLE.....                              | DET          | NORMAL CROWN SLOPE.....         | NCS        | SUB-GRADE.....                            | SG                      | PROPERTY                                    |             |
| DETAIL.....                                  | DTL          | NORTH.....                      | N          | SUPERELEVATION.....                       | SE, se or e             | DEED BOOK.....                              | DB          |
| DIAMETER.....                                | DIA          | NORTH BOUND.....                | NB         | SURVEY.....                               | SRV                     | REAL PROPERTY BOOK.....                     | RP          |
| DIRECTION.....                               | DIR          | NORTH BOUND ROADWAY.....        | NBR        | SYMMETRICAL.....                          | SYM                     | PLAT BOOK.....                              | PB          |
| DISTANCE.....                                | DIST         | NORTHING-EASTING.....           | NE         | TANGENT.....                              | TAN                     | MAP BOOK.....                               | MB          |
| DOUBLE.....                                  | DBL          | NOT IN CONTRACT.....            | NIC        | TANGENT LENGTH (CURVE DATA).....          | T                       | PAGE.....                                   | PG          |
| DOUBLE BARREL CULVERT.....                   | CD           | NOT TO SCALE.....               | NTS        | TANGENT TO CURVE.....                     | TC                      | OFFICIAL RECORD.....                        | OR          |
| DRAINAGE AREA.....                           | DA           | NUMBER.....                     | NO         | TANGENT TO SPIRAL.....                    | TS                      | CAPPED (TYPICAL PLASTIC SURVEYORS CAP)..... | CAP         |
| DRIVE.....                                   | DR           | OBSERVATION.....                | OBS        | TEMPORARY.....                            | TEMP                    | ALUMINUM CAP.....                           | ALUM CAP    |
| DROP INLET.....                              | DI           | ON CENTER.....                  | OC         | TEMPORARY BENCH MARK.....                 | TBM                     | BRASS CAP.....                              | BR CAP      |
| EACH.....                                    | EA           | ORIGINAL.....                   | ORIG       | TEMPORARY CONSTRUCTION EASEMENT.....      | TCE                     | IRON PIPE.....                              | IP          |
| EASEMENT.....                                | ESMT         | OVERHEAD.....                   | OHD        | THROAT.....                               | TH                      | CRIMPED.....                                | CR          |
| EAST.....                                    | E            | OVERHAUL.....                   | OH         | TOWNSHIP.....                             | TSHP                    | REINFORCING STEEL.....                      | REBAR       |
| EAST BOUND.....                              | EB           | OUT TO OUT.....                 | OO         | TRANSITION.....                           | TRANS                   | CONCRETE MONUMENT.....                      | CM          |
| EAST BOUND ROADWAY.....                      | EBR          | PAINT.....                      | PNT        | TRIPLE.....                               | TR                      | DAMAGED.....                                | DAM         |
| EDGE OF PAVEMENT.....                        | EP           | PAVED.....                      | PVD        | TRIPLE BARREL CULVERT.....                | CT                      | CHISELED X.....                             | CH" X"      |
| ELEVATION.....                               | EL or ELEV   | PAVED SHOULDER.....             | PVD SH     | TURN OUT.....                             | TO                      | HUB AND TACK.....                           | H&T         |
| END OF RETURN.....                           | ER           | PAVEMENT.....                   | PVMT       | TURNING POINT.....                        | TP                      | NAIL AND BOTTLE TOP.....                    | N&BT        |
| END ANCHOR.....                              | E/A          | PIPE END TREATMENT.....         | PET        | TYPE.....                                 | TY                      | PARKER-KALON (MASONARY NAILS).....          | PK NAIL     |
| END OF PROJECT.....                          | EOP          | PIPE ENTERING CULVERT.....      | PEC        | UNIT.....                                 | U                       | FENCE POST.....                             | F-POST      |
| EQUATION.....                                | EQ           | PLATE GIRDER.....               | PL GDR     | UNKNOWN.....                              | UNK                     | RAILROAD IRON.....                          | RR IRON     |
| EROSION CONTROL PRODUCTS.....                | ECP          | POINT OF BEGINNING.....         | POB        | UNPAVED.....                              | UNPVD                   | COTTON SPINDLE.....                         | COT SP      |
| EXCAVATION.....                              | EXCAV        | POINT OF COMPOUND CURVE.....    | PCC        | VALLEY GUTTER.....                        | VG                      | ANGLE IRON.....                             | ANGLE IRON  |
| EXISTING.....                                | EX           | POINT OF CURVATURE.....         | PC         | VARIABLE.....                             | VAR                     |   |             |
| EXPANSION.....                               | EXP          | POINT OF REVERSE CURVATURE..... | PRC        | VERTICAL.....                             | VERT                    |   |             |
| EXTENSION.....                               | EXT          | POINT OF ENDING.....            | POE        | VERTICAL CURVE.....                       | VC                      |   |             |
| EXTERNAL.....                                | EXT          | POINT OF INTERSECTION.....      | PI         | VERTICAL POINT OF CURVATURE.....          | PVC                     |   |             |
| EXTRA STRENGTH.....                          | EXT STR      | POINT OF TANGENCY.....          | PT         | VERTICAL POINT OF INTERSECTION.....       | PVI                     |   |             |
| FEET.....                                    | FT           | POINT ON CURVE.....             | POC        | VERTICAL POINT OF TANGENCY.....           | PVT                     |   |             |
| FILTER.....                                  | F            | POUND.....                      | LB         | VITRIFIED.....                            | VIT                     |   |             |
| FILTER BLANKET.....                          | FLT BLNK     | PRESENT.....                    | PRES       | VOLUME.....                               | VOL                     |   |             |
| FINISHED GRADE.....                          | FG           | PROFILE GRADE.....              | PG         | WEST.....                                 | W                       |   |             |
| FINISHED SURFACE.....                        | FYS          | PROJECT.....                    | PROJ       | WEST BOUND.....                           | WB                      |   |             |
| FISCAL YEAR.....                             | FY           | PROJECT CONTROL.....            | PJC        | WEST BOUND ROADWAY.....                   | WBR                     |   |             |
| FIXED.....                                   | FIX          | PROPERTY LINE.....              | PL         | WING WALL.....                            | WW                      |   |             |

REV 8/04/2015 - ALDOT ABBREVIATIONS DATED 4/6/2015 WITH THE FOLLOWING ADDITIONS:  
BOLLARD (B), BRIDGE IDENTIFICATION NUMBER (BIN), CONSTRUCTION  
(CONST), DESIGN SPEED (V), DETACHABLE (DET), DETAIL (DTL),  
EAST BOUND (EB), NORTH BOUND (NB), SOUTH BOUND (SB), TANGENT TO  
CURVE (TC), TRANSITION (TRANS), WEST BOUND (WB), AND WITH (W/).

PRIMARY SURVEY CONTROL AND GEOMETRIC LAYOUT SHEET

|                         |                |             |
|-------------------------|----------------|-------------|
| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
| SCP 59-889-18           | 2023           | 1-F         |



| HORIZONTAL AND VERTICAL CONTROL POINTS |          |          |             |             |           |             |
|--|----------|----------|-------------|-------------|-----------|-------------|
| POINT NO.                              | STATION  | OFFSET   | NORTHING    | EASTING     | ELEVATION | DESCRIPTION |
| 1                                      | 10+27.56 | 5.46 RT  | 1181385.890 | 2166124.040 | 418.780   | CAPPED IRON |
| 2                                      | 10+57.22 | 37.08 LT | 1181425.420 | 2166090.470 | 419.510   | CAPPED IRON |
| 3                                      | 16+67.74 | 74.88 LT | 1262956.330 | 2166251.270 | 418.480   | CAPPED IRON |

- NOTES:
- POINT COORDINATES ARE BASED ON THE HORIZONTAL DATUM-NAD 83/92 (HPGN) ALABAMA STATE WEST ZONE, VERTICAL DATUM-NAVD 88, AND U.S. SURVEY FOOT UNIT OF MEASURE WITH AN AVERAGE COMBINED SCALE FACTOR OF 0.99996288906523.
  - SEE SHEET 1-G FOR THE HORIZONTAL ALIGNMENT DATA SHEET.

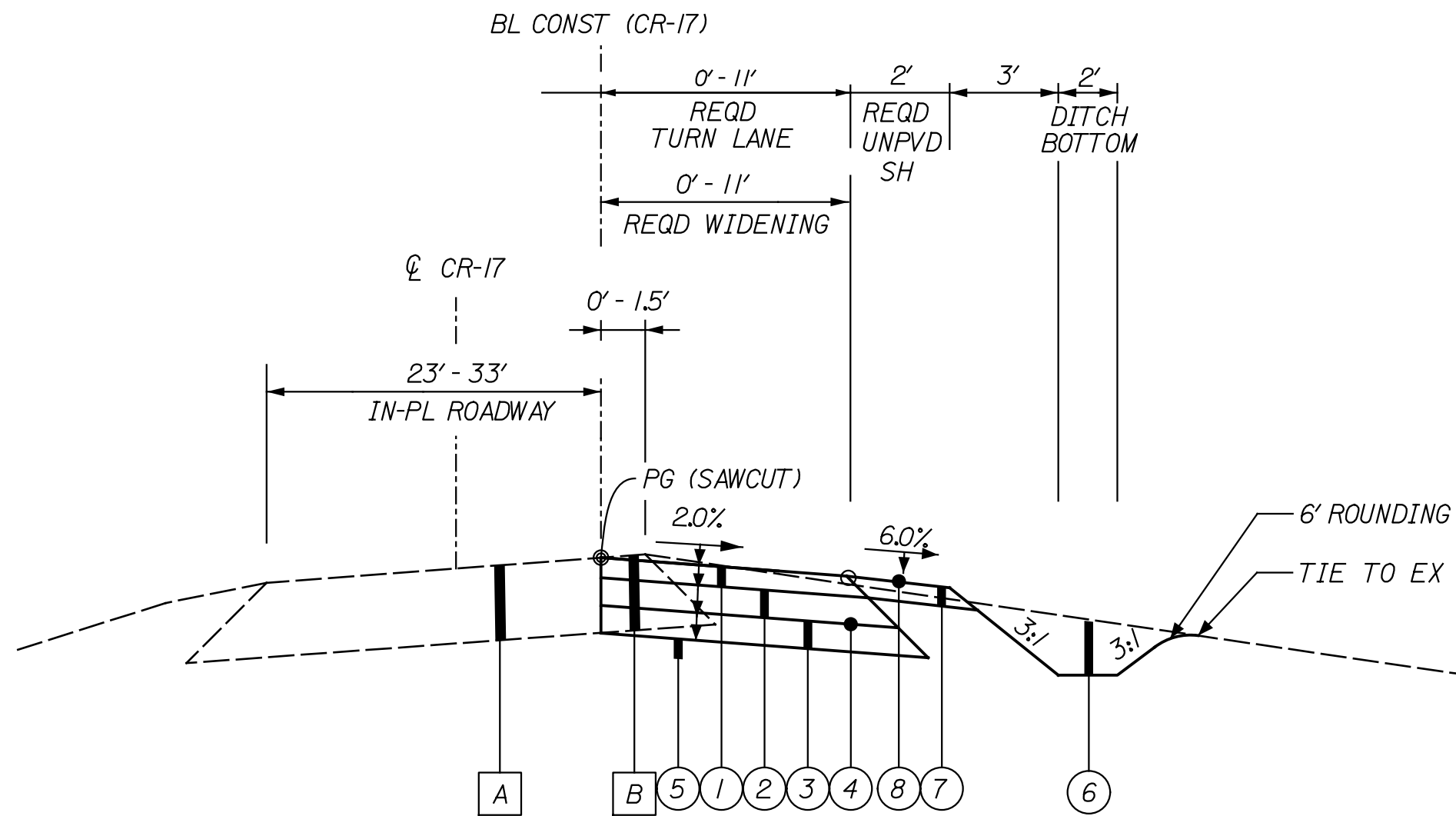
|  |                                      |                                    |                             |                |   |   |               |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|---|---|---------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | <div>HORIZ</div> <div><div>50050</div><div></div></div> <div>SCALE ( FEET )</div> | SHEET TITLE                                       | ROUTE         |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |   | PRIMARY SURVEY CONTROL AND GEOMETRIC LAYOUT SHEET | CR-17 @ CR-44 |

| GEOMETRIC LAYOUT SHEET   |                                      |                                    |                             |                |  |              | REFERENCE<br>PROJECT NO | FISCAL<br>YEAR   | SHEET<br>NO |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|--|--------------|-------------------------|------------------|-------------|---------|----------|---------|-----------------|--|--|--|----------|----------|--------------|--------------|---------|----------|--------------|--------------|--------------------|---------------|--|--|-----------------|--------|--|--|-------------------|--|--|--|---------|----------|--------------|--------------|--------|----------|--------------|--------------|---------|--|--------------|--------------|-----------|----------|--------------|--------------|---------|---------|--|--|--------|---------------|--|--|---------------------------|----------|--|--|---------|--------|--|--|----------|-------|--|--|--------|--------|--|--|------------------|------|--|--|-----------|------|--|--|--------------------|---------------|--|--|-------------------|---------------|--|--|------------------|---------------|--|--|-------------------|---------------|--|--|--------------------|---------------|--|--|-------------------|--|--|--|-----------|----------|--------------|--------------|--------|----------|--------------|--------------|----------|--|--------------|--------------|----------|----------|--------------|--------------|---------|-------|--|--|--------|------------------|--|--|---------------------------|------------|--|--|---------|-------|--|--|----------|-------|--|--|--------|-------|--|--|------------------|-------|--|--|-----------|-------|--|--|--------------------|---------------|--|--|-------------------|---------------|--|--|------------------|---------------|--|--|-------------------|---------------|--|--|--------------------|---------------|--|--|-----------------|--|--|--|----------|----------|--------------|--------------|-----------|----------|--------------|--------------|--------------------|---------------|--|--|-----------------|-------|--|--|
|  |                                      |                                    |                             |                |  |              | SCP 59-889-18           | 2023             | 1-G         |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
|  | HORIZONTAL ALIGNMENT DATA SHEET      |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| <div>Project Name: CO RD 17<br/>Description:<br/>Horizontal Alignment Name: CR-17 EOWidening<br/>Description:<br/>Style: NBR</div> <table><thead><tr><th></th><th>STATION</th><th>NORTHING</th><th>EASTING</th></tr></thead><tbody><tr><td colspan="4">Element: Linear</td></tr><tr><td>PDB ( 8)</td><td>10+00.00</td><td>1181360.6357</td><td>2166111.7390</td></tr><tr><td>PC ( 9)</td><td>15+05.54</td><td>1181849.4666</td><td>2166240.6345</td></tr><tr><td>Tangent Direction:</td><td>N 14°46'18" E</td><td></td><td></td></tr><tr><td>Tangent Length:</td><td>505.54</td><td></td><td></td></tr><tr><td colspan="4">Element: Circular</td></tr><tr><td>PC ( 9)</td><td>15+05.54</td><td>1181849.4666</td><td>2166240.6345</td></tr><tr><td>PI ( )</td><td>15+67.20</td><td>1181909.0902</td><td>2166256.3562</td></tr><tr><td>CC ( 4)</td><td></td><td>1182808.3959</td><td>2162603.9361</td></tr><tr><td>PRC ( 10)</td><td>16+28.85</td><td>1181969.1972</td><td>2166270.1148</td></tr><tr><td>Radius:</td><td>3761.00</td><td></td><td></td></tr><tr><td>Delta:</td><td>1°52'43" Left</td><td></td><td></td></tr><tr><td>Degree of Curvature(Arc):</td><td>1°31'24"</td><td></td><td></td></tr><tr><td>Length:</td><td>123.31</td><td></td><td></td></tr><tr><td>Tangent:</td><td>61.66</td><td></td><td></td></tr><tr><td>Chord:</td><td>123.31</td><td></td><td></td></tr><tr><td>Middle Ordinate:</td><td>0.51</td><td></td><td></td></tr><tr><td>External:</td><td>0.51</td><td></td><td></td></tr><tr><td>Tangent Direction:</td><td>N 14°46'18" E</td><td></td><td></td></tr><tr><td>Radial Direction:</td><td>S 75°13'42" E</td><td></td><td></td></tr><tr><td>Chord Direction:</td><td>N 13°49'56" E</td><td></td><td></td></tr><tr><td>Radial Direction:</td><td>S 77°06'25" E</td><td></td><td></td></tr><tr><td>Tangent Direction:</td><td>N 12°53'35" E</td><td></td><td></td></tr><tr><td colspan="4">Element: Circular</td></tr><tr><td>PRC ( 10)</td><td>16+28.85</td><td>1181969.1972</td><td>2166270.1148</td></tr><tr><td>PI ( )</td><td>16+89.47</td><td>1182028.2920</td><td>2166283.6418</td></tr><tr><td>CC ( 11)</td><td></td><td>1181957.9616</td><td>2166319.1993</td></tr><tr><td>PT ( 12)</td><td>17+17.24</td><td>1182004.1509</td><td>2166339.2510</td></tr><tr><td>Radius:</td><td>50.35</td><td></td><td></td></tr><tr><td>Delta:</td><td>100°34'25" Right</td><td></td><td></td></tr><tr><td>Degree of Curvature(Arc):</td><td>113°47'10"</td><td></td><td></td></tr><tr><td>Length:</td><td>88.39</td><td></td><td></td></tr><tr><td>Tangent:</td><td>60.62</td><td></td><td></td></tr><tr><td>Chord:</td><td>77.47</td><td></td><td></td></tr><tr><td>Middle Ordinate:</td><td>18.18</td><td></td><td></td></tr><tr><td>External:</td><td>28.45</td><td></td><td></td></tr><tr><td>Tangent Direction:</td><td>N 12°53'35" E</td><td></td><td></td></tr><tr><td>Radial Direction:</td><td>S 77°06'25" E</td><td></td><td></td></tr><tr><td>Chord Direction:</td><td>N 63°10'47" E</td><td></td><td></td></tr><tr><td>Radial Direction:</td><td>S 23°28'00" W</td><td></td><td></td></tr><tr><td>Tangent Direction:</td><td>S 66°32'00" E</td><td></td><td></td></tr><tr><td colspan="4">Element: Linear</td></tr><tr><td>PT ( 12)</td><td>17+17.24</td><td>1182004.1509</td><td>2166339.2510</td></tr><tr><td>PDE ( 13)</td><td>17+32.30</td><td>1181998.1553</td><td>2166353.0620</td></tr><tr><td>Tangent Direction:</td><td>S 66°32'00" E</td><td></td><td></td></tr><tr><td>Tangent Length:</td><td>15.06</td><td></td><td></td></tr></tbody></table> <div>NOTES:<br/>1. POINT COORDINATES ARE BASED ON THE HORIZONTAL DATUM-NAD 83/92 (HPGN)<br/>ALABAMA STATE WEST ZONE, VERTICAL DATUM-NAVD 88, AND U.S. SURVEY FOOT<br/>UNIT OF MEASURE WITH AN AVERAGE COMBINED SCALE FACTOR OF 0.99996288906523.</div> |                                      |                                    |                             |                |  |              |                         |                  |             | STATION | NORTHING | EASTING | Element: Linear |  |  |  | PDB ( 8) | 10+00.00 | 1181360.6357 | 2166111.7390 | PC ( 9) | 15+05.54 | 1181849.4666 | 2166240.6345 | Tangent Direction: | N 14°46'18" E |  |  | Tangent Length: | 505.54 |  |  | Element: Circular |  |  |  | PC ( 9) | 15+05.54 | 1181849.4666 | 2166240.6345 | PI ( ) | 15+67.20 | 1181909.0902 | 2166256.3562 | CC ( 4) |  | 1182808.3959 | 2162603.9361 | PRC ( 10) | 16+28.85 | 1181969.1972 | 2166270.1148 | Radius: | 3761.00 |  |  | Delta: | 1°52'43" Left |  |  | Degree of Curvature(Arc): | 1°31'24" |  |  | Length: | 123.31 |  |  | Tangent: | 61.66 |  |  | Chord: | 123.31 |  |  | Middle Ordinate: | 0.51 |  |  | External: | 0.51 |  |  | Tangent Direction: | N 14°46'18" E |  |  | Radial Direction: | S 75°13'42" E |  |  | Chord Direction: | N 13°49'56" E |  |  | Radial Direction: | S 77°06'25" E |  |  | Tangent Direction: | N 12°53'35" E |  |  | Element: Circular |  |  |  | PRC ( 10) | 16+28.85 | 1181969.1972 | 2166270.1148 | PI ( ) | 16+89.47 | 1182028.2920 | 2166283.6418 | CC ( 11) |  | 1181957.9616 | 2166319.1993 | PT ( 12) | 17+17.24 | 1182004.1509 | 2166339.2510 | Radius: | 50.35 |  |  | Delta: | 100°34'25" Right |  |  | Degree of Curvature(Arc): | 113°47'10" |  |  | Length: | 88.39 |  |  | Tangent: | 60.62 |  |  | Chord: | 77.47 |  |  | Middle Ordinate: | 18.18 |  |  | External: | 28.45 |  |  | Tangent Direction: | N 12°53'35" E |  |  | Radial Direction: | S 77°06'25" E |  |  | Chord Direction: | N 63°10'47" E |  |  | Radial Direction: | S 23°28'00" W |  |  | Tangent Direction: | S 66°32'00" E |  |  | Element: Linear |  |  |  | PT ( 12) | 17+17.24 | 1182004.1509 | 2166339.2510 | PDE ( 13) | 17+32.30 | 1181998.1553 | 2166353.0620 | Tangent Direction: | S 66°32'00" E |  |  | Tangent Length: | 15.06 |  |  |
|  | STATION                              | NORTHING                           | EASTING                     |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Element: Linear  |                                      |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PDB ( 8)   | 10+00.00                             | 1181360.6357                       | 2166111.7390                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PC ( 9)  | 15+05.54                             | 1181849.4666                       | 2166240.6345                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Direction:   | N 14°46'18" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Length:  | 505.54                               |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Element: Circular  |                                      |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PC ( 9)  | 15+05.54                             | 1181849.4666                       | 2166240.6345                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PI ( )   | 15+67.20                             | 1181909.0902                       | 2166256.3562                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| CC ( 4)  |                                      | 1182808.3959                       | 2162603.9361                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PRC ( 10)  | 16+28.85                             | 1181969.1972                       | 2166270.1148                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Radius:  | 3761.00                              |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Delta:   | 1°52'43" Left                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Degree of Curvature(Arc):  | 1°31'24"                             |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Length:  | 123.31                               |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent:   | 61.66                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Chord:   | 123.31                               |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Middle Ordinate:   | 0.51                                 |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| External:  | 0.51                                 |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Direction:   | N 14°46'18" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Radial Direction:  | S 75°13'42" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Chord Direction:   | N 13°49'56" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Radial Direction:  | S 77°06'25" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Direction:   | N 12°53'35" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Element: Circular  |                                      |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PRC ( 10)  | 16+28.85                             | 1181969.1972                       | 2166270.1148                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PI ( )   | 16+89.47                             | 1182028.2920                       | 2166283.6418                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| CC ( 11)   |                                      | 1181957.9616                       | 2166319.1993                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PT ( 12)   | 17+17.24                             | 1182004.1509                       | 2166339.2510                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Radius:  | 50.35                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Delta:   | 100°34'25" Right                     |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Degree of Curvature(Arc):  | 113°47'10"                           |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Length:  | 88.39                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent:   | 60.62                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Chord:   | 77.47                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Middle Ordinate:   | 18.18                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| External:  | 28.45                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Direction:   | N 12°53'35" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Radial Direction:  | S 77°06'25" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Chord Direction:   | N 63°10'47" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Radial Direction:  | S 23°28'00" W                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Direction:   | S 66°32'00" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Element: Linear  |                                      |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PT ( 12)   | 17+17.24                             | 1182004.1509                       | 2166339.2510                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| PDE ( 13)  | 17+32.30                             | 1181998.1553                       | 2166353.0620                |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Direction:   | S 66°32'00" E                        |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
| Tangent Length:  | 15.06                                |                                    |                             |                |  |              |                         |                  |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  | NOT TO SCALE | SHEET TITLE             | ROUTE            |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |  |              | GEOMETRIC LAYOUT SHEET  | CR-17 @<br>CR-44 |             |         |          |         |                 |  |  |  |          |          |              |              |         |          |              |              |                    |               |  |  |                 |        |  |  |                   |  |  |  |         |          |              |              |        |          |              |              |         |  |              |              |           |          |              |              |         |         |  |  |        |               |  |  |                           |          |  |  |         |        |  |  |          |       |  |  |        |        |  |  |                  |      |  |  |           |      |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                   |  |  |  |           |          |              |              |        |          |              |              |          |  |              |              |          |          |              |              |         |       |  |  |        |                  |  |  |                           |            |  |  |         |       |  |  |          |       |  |  |        |       |  |  |                  |       |  |  |           |       |  |  |                    |               |  |  |                   |               |  |  |                  |               |  |  |                   |               |  |  |                    |               |  |  |                 |  |  |  |          |          |              |              |           |          |              |              |                    |               |  |  |                 |       |  |  |

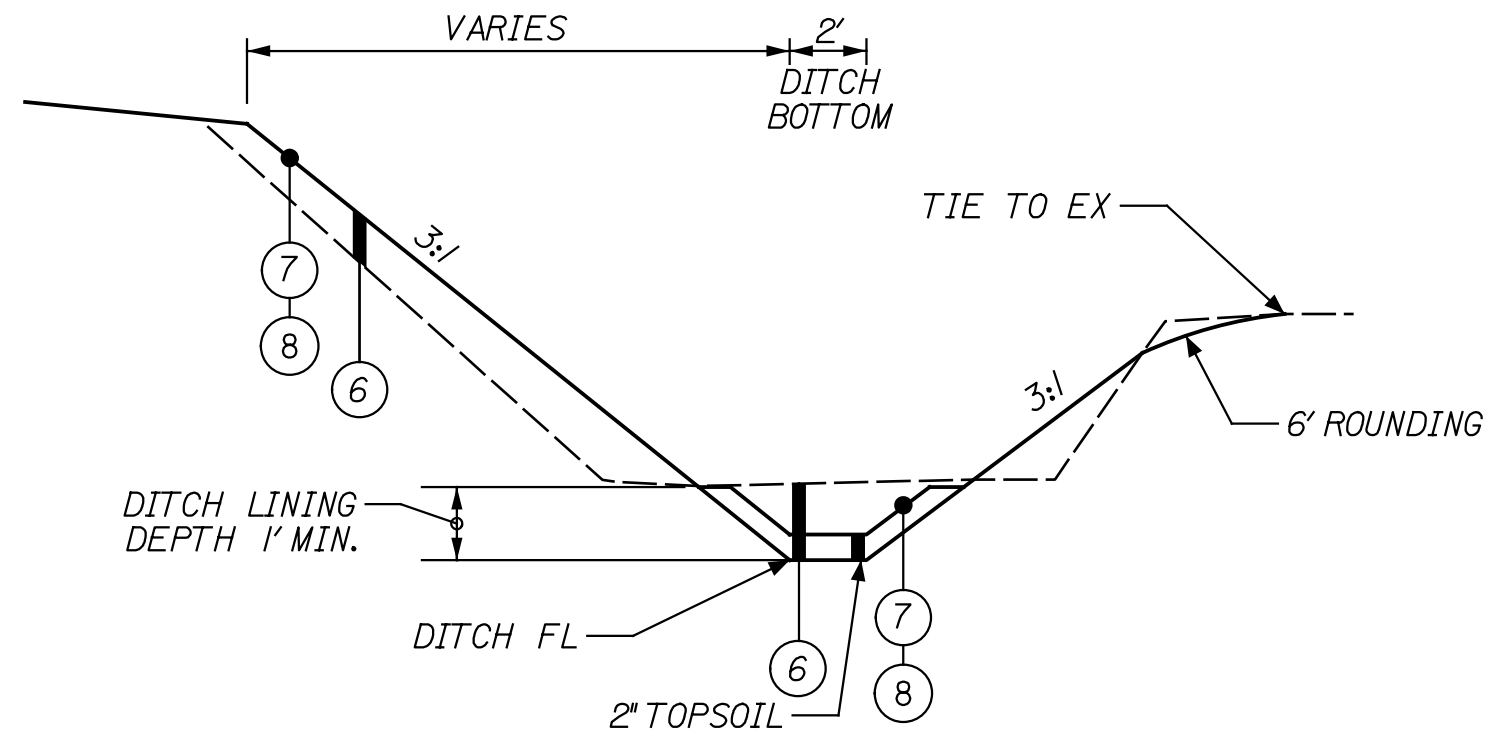
TYPICAL SECTION SHEET

|                         |                |             |
|-------------------------|----------------|-------------|
| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
| SCP 59-889-18           | 2023           | 2           |

PROJECT NOTES  
200 - 205



TYPICAL SECTION FOR CR-17  
STA 11+18.10 TO STA 17+32.30



SPECIAL DITCH TYPICAL

STA 11+80 TO STA 12+50 RT  
STA 14+50 TO STA 17+00 RT  
STA 13+60 TO STA 13+80 RT

| REQUIRED MATERIALS LEGEND |                          |   |
|---------------------------|--------------------------|---|
| DESCRIPTION               |                          |   |
| LEGEND NO.                | ITEM NO.                 |   |
| ①                         | 424A-361                 | SUPERPAVE BITUMINOUS CONCRETE WEARING SURFACE LAYER, 3/4" MAXIMUM AGGREGATE SIZE MIX, ESAL RANGE C/D (APPROXIMATELY 185 LB/SQ YD) |
| ②                         | 424B-650                 | SUPERPAVE BITUMINOUS CONCRETE UPPER BINDER LAYER, 3/4" MAXIMUM AGGREGATE SIZE MIX, ESAL RANGE C/D (APPROXIMATELY 250 LB/SQ YD)    |
| ③                         | 301A-020                 | CRUSHED AGGREGATE BASE COURSE, TYPE B, PLANT MIXED 8" COMPACTED THICKNESS (WIDTH VARIES 0' - 11.5')                               |
| ④                         | 401A-000                 | BITUMINOUS TREATMENT A (WIDTH VARIES 0' - 11.5')  |
| ⑤                         | 230A-000                 | ROADBED PROCESSING (WIDTH VARIES 6' - 11.5')  |
| ⑥                         | 210A-000 AND/OR 210D-011 | UNCLASSIFIED EXCAVATION AND/OR BORROW EXCAVATION (A4 OR BETTER)   |
| ⑦                         | 650A-000                 | TOPSOIL (4" THICK)  |
| ⑧                         | 654A-001                 | SOLID SODDING (BERMUDA)   |

NOTE: PER SHELBY COUNTY STANDARDS, ALL WIDENING AREAS LESS THAN 4' WIDE SHALL BE FULL DEPTH ASPHALT.

| IN PLACE MATERIALS LEGEND |                                       |
|---------------------------|---------------------------------------|
| LEGEND NO.                | DESCRIPTION                           |
| A                         | EXISTING BITUMINOUS PAVEMENT (RETAIN) |
| B                         | EXISTING BITUMINOUS PAVEMENT (REMOVE) |

|  |                                      |                                    |                             |                |              |                       |               |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|--------------|-----------------------|---------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | NOT TO SCALE | SHEET TITLE           | ROUTE         |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |              | TYPICAL SECTION SHEET | CR-17 @ CR-44 |



# PROJECT NOTE SHEET

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 2-A         |

| SERIES<br>NOTE | NOTE  |
|----------------|---|
| 200            | THE PAVING LAYOUT SHEETS SHALL BE USED IN CONJUNCTION WITH THE TYPICAL SECTIONS FOR PAVEMENT WIDTH TRANSITIONS, RADII LENGTHS, ACCELERATION AND DECELERATION LANES, TAPER LENGTHS, ETC.   |
| 201            | ALL EXISTING GRAVEL, DIRT, AND ASPHALT DRIVEWAYS AND TURNOUTS SHALL BE RESURFACED FOR A DISTANCE OF 3 FEET, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.  |
| 202            | ROADBED PROCESSING FOR THE SUBGRADE IN AREAS 6 FEET WIDE OR LESS SHALL BE WAIVED. THE DENSITY OF SUBGRADE IN THESE AREAS SHALL BE TO 100% COMPACTION MEETING AASHTO T-99 STANDARDS, AND THE PAYMENT OF THIS WORK SHALL BE A SUBSIDIARY OBLIGATION OF THE OVERLYING LAYER. |
| 203            | JOB MIX FORMULA AND L.A. ABRASION DATA ARE NOT AVAILABLE FOR THIS PROJECT.  |
| 204            | FOR WIDENING AREAS FOUR FEET WIDE AND LESS, THE PAVEMENT BUILDUP SHALL BE FULL DEPTH ASPHALT, WITH UPPER BINDER REPLACING THE CRUSHED AGGREGATE LAYER.  |
| 205            | ALL REFERENCES TO "ALDOT" SHOWN ON THE REQUIRED RIGHT-OF-WAY MARKERS AS DETAILED ON STANDARD DRAWING M-602 SHALL BE CHANGED TO "SHELBY COUNTY HWY DEPT". THE COST OF THIS WORK SHALL BE A SUBSIDIARY OBLIGATION OF THE PAY ITEM 602A-000.                                 |
| 300            | TEMPORARY PAVEMENT MARKERS SHALL BE OF THE PERMANENT TYPE, MEETING THE REQUIREMENTS OF SUBARTICLE 882.02 (b) OF THE STANDARD SPECIFICATIONS.  |
| 301            | ITEMS TO BE REMOVED THAT ARE NOT IDENTIFIED BEFORE CONSTRUCTION SHALL BE REMOVED IN ACCORDANCE WITH THE REQUIREMENTS FOR EXTRA WORK GIVEN IN ARTICLE 104.03 OF THE STANDARD SPECIFICATIONS.   |
| 302            | PAY ITEM NO. 654A-001 (SOLID SODDING (BERMUDA)) SHALL BE PLACED IN DITCH BOTTOMS.   |

| SERIES<br>NOTE | NOTE  |
|----------------|---|
| 400            | OMIT  |
| 401            | OMIT  |
| 800            | IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY OWNERS AND DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES ON THIS PROJECT WHETHER SHOWN ON THE PLANS OR NOT. THE LOCATION OF ANY REQUIRED GUARDRAIL, SIGNS, FOOTINGS OF ANY NATURE AND/ OR ELECTRICAL/ COMMUNICATIONS CONDUITS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO PREVENT ANY CONFLICTS WITH THESE UTILITIES. UTILITY LINE LOCATE RE-QUESTS SHALL BE LIMITED TO INCREMENTS NOT TO EXCEED 2000 LINEAR FEET PER WORKING DAY OPERATIONS. MULTIPLE LOCATE REQUESTS SHALL BE REQUIRED FOR PROJECTS GREATER THAN 2000 LINEAR FEET IN LENGTH. |
| 900            | NPDES PERMIT COVERAGE IS NOT REQUIRED FOR THIS PROJECT.   |
| 901            | THERE SHALL BE NO FUEL TANKS STORED ON THE RIGHT OF WAY. IN ADDITION, FUEL TRUCKS OR VEHICLES TRANSPORTING CHEMICALS, FERTILIZERS, ETC. SHALL NOT BE LEFT UNATTENDED ON THE RIGHT OF WAY.   |

|  |                                      |                                    |                             |                |  |              |                    |                  |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|--|--------------|--------------------|------------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  | NOT TO SCALE | SHEET TITLE        | ROUTE            |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |  |              | PROJECT NOTE SHEET | CR-17 @<br>CR-44 |

| GENERAL TRAFFIC CONTROL PLAN NOTES                    |  |  |  |                |   | REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO    |  |
|---|--|--|--|----------------|---|-------------------------|----------------|----------------|--|
|   |  |  |  |                |   | SCP 59-889-18           | 2023           | 2-B            |  |
| <div>○ DENOTES NOTES THAT APPLY TO THIS PROJECT</div> |  |  |  |                |   |                         |                |                |  |
| <div>700</div>  | THE TRAFFIC CONTROL PLAN IS DEVELOPED IN CONFORMANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES PART 6, 2009 EDITION. THE TRAFFIC CONTROL DEVICES INDICATED REPRESENT CONDITIONS KNOWN DURING PLAN DEVELOPMENT. IN THE EVENT ACTUAL PHYSICAL CONDITIONS WARRANT ADDITIONAL TRAFFIC CONTROL DEVICES, THEY SHALL BE INSTALLED IN CONFORMANCE WITH THE M.U.T.C.D. PART 6 AS DIRECTED BY THE ENGINEER. COST SHALL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.  |  |  | 719            | OMITTED   |                         |                | 748            | R16-3 AND R16-3a SIGNS SHALL BE REQUIRED FOR EVERY PROJECT ON STATE ROUTES AND INTERSTATE HIGHWAYS. THEY SHALL BE POSTED AT THE BEGINNING AND END OF THE PROJECT. AN R2-1 SIGN SHALL ALWAYS BE REQUIRED FOLLOWING AN R16-3 SIGN. ADDITIONAL R16-3 SIGNS SHALL BE POSTED AT MAXIMUM THREE MILE INTERVALS THROUGHOUT THE PROJECT LIMITS. ADDITIONAL R16-3 SIGNS SHALL BE REQUIRED WITH A W3-5b SIGN AND R2-1 SIGN WHENEVER A REDUCTION IN SPEED IS REQUIRED.   |
| <div>701</div>  | ALL BLACK ON ORANGE CONSTRUCTION SIGNS SHALL BE FABRICATED USING TYPES IV OR VIII FLUORESCENT ORANGE REFLECTIVE SHEETING MATERIAL FOR THE SIGN BACKGROUND.   |  |  | 721            | OMITTED   |                         |                |                |  |
| <div>702</div>  | DURING NON-WORKING HOURS NO EQUIPMENT OR MATERIAL SHALL BE PARKED OR STORED CLOSER THAN 30 FEET TO THE EDGE OF ANY ROADWAY CARRYING TRAFFIC. WHEN THIS IS NOT PRACTICAL, IT SHALL BE PLACED IN AN AREA APPROVED BY THE ENGINEER AND DELINEATED BY REFLECTORIZED DRUMS. THIS INCLUDES STORAGE OF TRAFFIC CONTROL DEVICES SUCH AS TRAILER MOUNTED OR OTHER TEMPORARY SIGNS, BARRICADES, DRUMS, ETC., WHICH ARE NOT IN USE DURING NON-WORKING HOURS. TO BE FURNISHED BY THE CONTRACTOR WITHOUT COST TO SHELBY CO. (SEE SKETCH ON SHEET 12)        |  |  | 722            | OMITTED   |                         |                |                |  |
| <div>703</div>  | WHERE THE LOCATION OF A REQUIRED SIGN FALLS IN A DRIVEWAY, SIDEWALK, BRIDGE, ETC. OR WHERE THE VISIBILITY OF A SIGN IS LIMITED TO THE TRAVELING PUBLIC, THE LOCATION SHALL BE ADJUSTED AS DIRECTED BY THE ENGINEER.  |  |  | 723            | THE CONTRACTOR SHALL MAKE PROVISIONS FOR THE SAFETY OF PEDESTRIAN TRAFFIC CROSSING THE WORK ZONES DURING CONSTRUCTION.  |                         |                | <div>749</div> | WHEN A CONSTRUCTION WORK ZONE SPEED LIMIT REDUCTION IS NOT REQUIRED AT THE END OF THE WORK DAY, THE CONTRACTOR SHALL COVER OR REMOVE THE REDUCED R2-1 (REGULATORY SPEED SIGNS) AND THE W3-5b (REDUCED SPEED AHEAD) SIGNS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.  |
| <div>704</div>  | THE CONTRACTOR IS TO REMOVE, RELOCATE OR COVER DURING CONSTRUCTION AND THEN RESET OR UNCOVER UPON COMPLETION OF A PARTICULAR SECTION ANY CONFLICTING IN-PLACE ROADWAY SIGNS AND DELINEATORS, AS DIRECTED BY THE ENGINEER. SIGNS REQUIRING REMOVAL SHALL BE STOCKPILED AS DIRECTED BY THE ENGINEER AND SHALL BECOME PROPERTY OF THE SHELBY CO. COST SHALL BE A SUBSIDIARY OBLIGATION OF ITEM 740B.  |  |  | 724            | OMITTED   |                         |                |                |  |
| <div>705</div>  | DURING ALL PHASES OF WORK, NON-APPLICABLE PAVEMENT STRIPING OR MARKINGS SHALL BE REMOVED AND APPROPRIATE PAVEMENT STRIPING OR MARKINGS SHALL BE PLACED AS EXPEDITIOUSLY AS PRACTICAL, BUT IN ALL CASES, SHALL BE IN PLACE BY NIGHTFALL ON ANY ROADWAY CARRYING TRAFFIC, EXCEPT ON SHORT TERM OPERATIONS WHERE IT IS DETERMINED BY THE ENGINEER, THAT SUCH REMOVAL AND REPLACEMENT IS MORE HAZARDOUS THAN LEAVING EXISTING MARKINGS IN PLACE. COST OF ANY REMOVAL SHALL BE PAID FOR UNDER ITEM 701D OR AS A SUBSIDIARY OBLIGATION OF ITEM 701C. |  |  | <div>725</div> | ALL SIGNS SHALL BE POST-MOUNTED IF THE WORK PERIOD EXCEEDS FOUR DAYS, EXCEPT FOR THOSE SIGNS WHICH ARE MOUNTED ON BARRICADES. FOR REPEATED DAY OPERATIONS, SIGNS MAY BE MOUNTED ON TEMPORARY SUPPORTS AND REMOVED AT THE COMPLETION OF THE DAY’S OPERATION.   |                         |                | 750            | DURING REPLACEMENT OF GUARDRAIL AND/OR GUARDRAIL END ANCHORS, A REFLECTORIZED DRUM WITH A LIGHTWEIGHT TYPE B WARNING LIGHT (WEIGHING 3.3 POUNDS OR LESS) SHALL BE PLACED BEFORE THE END OF ANY EXPOSED GUARDRAIL AT NIGHT WHERE THE GUARDRAIL END ANCHOR CANNOT BE REPLACED IN ONE DAY’S TIME.   |
| 706   | OMITTED  |  |  | 726            | OMITTED   |                         |                |                |  |
| <div>707</div>  | THE CONTRACTOR SHALL PLACE ALL ADVANCE WARNING SIGNS BEFORE PROCEEDING WITH HIS WORK. SIGNS SHALL BE PLACED IN ORDER, IN THE DIRECTION OF TRAFFIC AND REMOVED IN REVERSE ORDER.  |  |  | <div>727</div> | DURING THE WIDENING OR RESURFACING OF ANY ROADWAY CARRYING TRAFFIC, THE CONTRACTOR SHALL ADVISE THE MOTORISTS OF ANY EDGE OF PAVEMENT DROP-OFFS 3 INCHES OR GREATER BY PLACING SHOULDER DROP-OFF SIGNS EVERY 1/2 MILE BEGINNING PRIOR TO THE WIDENING OR RESURFACING. REQUIRED SHOULDER WORK TO ELIMINATE THE DROP-OFFS SHALL BE PURSUED IN AN EXPEDITIOUS MANNER FOLLOWING THE WIDENING AND/OR RESURFACING.                  |                         |                | <div>751</div> | CONSTRUCTION SIGNS MOUNTED ON A SINGLE OR DUAL SQUARE TUBULAR OR U-CHANNEL POST SHALL BE INSTALLED AS SHOWN ON SPECIAL DRAWING NOS. IHS-710-21 AND IHS-710-23.   |
| <div>708</div>  | ALL VEHICLES, EQUIPMENT, PERSONNEL (EXCEPT FLAGGERS), AND THEIR ACTIVITIES, ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.   |  |  | 728            | A DIFFERENCE IN ELEVATION OF APPROXIMATELY 2 INCHES OR LESS AT THE CENTERLINE MAY BE ALLOWED DURING NON-WORKING HOURS WITHOUT ADDITIONAL TRAFFIC CONTROL. SPECIAL CONDITIONS MAY EXIST WHERE PROTECTION SHOULD BE PROVIDED WHERE THE DIFFERENCE IS 2 INCHES OR LESS.  |                         |                | <div>752</div> | THE CONTRACTOR AND THE CONSTRUCTION ENGINEER SHALL DISCUSS AND PLAN FOR THE HANDLING OF TRAFFIC FOR ALL HOLIDAYS BEFORE ANY WORK BEGINS. UNLESS OTHERWISE PRE-APPROVED BY THE COUNTY ENGINEER, THE FOLLOWING SHALL HOLD:<br><br>THE CONTRACTOR SHALL NOT HAVE A LANE CLOSURE DURING THE FOLLOWING PERIODS UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR SHELBY CO:<br><br>FOR CHRISTMAS AND NEW YEARS DAY:<br>FROM 11:59 PM DECEMBER 23 THROUGH 11:59 PM JANUARY 2.<br><br>FOR NATIONAL MEMORIAL DAY AND LABOR DAY:<br>FROM 12:00 NOON THE FRIDAY BEFORE THE HOLIDAY THROUGH 11:59 PM THE DAY OF THE HOLIDAY.<br><br>FOR INDEPENDENCE DAY (THE 4TH OF JULY)<br>FROM 12:00 NOON THE DAY BEFORE THE HOLIDAY THROUGH 11:59 PM THE DAY OF THE HOLIDAY.<br><br>FOR THANKSGIVING DAY:<br>FROM 12:00 NOON THE WEDNESDAY BEFORE THANKSGIVING DAY THROUGH 11:59 PM THE SUNDAY FOLLOWING THANKSGIVING DAY.<br><br>ANY OTHER STATE HOLIDAYS WILL BE HANDLED AS APPROVED BY THE PROJECT ENGINEER.<br><br>THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER AND LOCAL GOVERNMENT ON TRAFFIC AND/OR WORK RESTRICTIONS FOR LOCAL HOLIDAYS OR EVENTS NOT LISTED ON ALDOT’S LIST OF OFFICIAL STATE HOLIDAYS. |
| 709   | THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE ACCESS TO BUSINESSES AND RESIDENCES DURING ALL PHASES OF CONSTRUCTION.  |  |  | <div>729</div> | SIGNS ON TEMPORARY SUPPORTS ARE TO BE REMOVED OR COVERED WHEN NO WORK IS BEING PERFORMED OR AT THE COMPLETION OF THE DAY’S OPERATION.   |                         |                |                |  |
| <div>710</div>  | CONSTRUCTION SIGNS MOUNTED ON TEMPORARY SUPPORTS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5 FEET.   |  |  | 730            | OMITTED   |                         |                |                |  |
| <div>711</div>  | FLAGGERS SHALL BE PROPERLY ATTIRED, EQUIPPED WITH STAFF MOUNTED STOP/SLOW PADDLES IN SIGHT OF EACH OTHER, OR HAVE DIRECT COMMUNICATION AT ALL TIMES. FLAGGER STATION LOCATION MAY BE VARIED FROM THOSE SHOWN BASED ON ROADWAY ALIGNMENT AND CONDITIONS AT THE TIME OF THE LANE CLOSURE.  |  |  | 731            | OMITTED   |                         |                |                |  |
| <div>712</div>  | FLAGGERS ARE TO BE USED WHEN DIRECTED BY THE ENGINEER. SIGNS SHALL BE PLACED AT THE APPROPRIATE TIME, AND SHALL BE COVERED OR REMOVED WHEN FLAGGERS ARE NOT ON DUTY AND DURING NON-WORKING HOURS.  |  |  | <div>732</div> | CHANNELIZING DRUMS SHOULD BE PLACED ON 10 FOOT INTERVALS IN RADII.  |                         |                |                |  |
| <div>713</div>  | FOR MOVING OPERATIONS, THE TRAFFIC CONES MAY BE DELETED IF THE FLAGGERS ARE IN SIGHT OF EACH OTHER, OR IF A PILOT CAR IS USED ON A TWO LANE ROADWAY.   |  |  | <div>733</div> | CHANNELIZING DRUMS PLACED TO PROTECT COMPLETED WORK NOT OPEN TO TRAFFIC, SHOULD BE SPACED AT 50 FOOT INTERVALS.   |                         |                |                |  |
| 714   | OMITTED  |  |  | <div>734</div> | CHANNELIZING DRUMS PLACED IN THE EXCAVATED AREA AHEAD OF PAVING OPERATIONS, SHOULD BE SPACED AT 50 FOOT INTERVALS.  |                         |                |                |  |
| <div>715</div>  | ALL CONTRACTOR’S EMPLOYEES’ PERSONAL VEHICLES, AND CONTRACTOR’S EQUIPMENT NOT IN OPERATION, SHALL BE PARKED A MINIMUM OF THIRTY (30) FEET FROM THE TRAVELED WAY DURING WORKING HOURS, AS NOT TO CREATE A HAZARD.   |  |  | <div>735</div> | CHANNELIZING DRUMS PLACED ON PAVEMENT DURING WORKING HOURS SHALL BE SHIFTED TO THE EDGE OF SHOULDER DURING NON-WORKING HOURS AND DURING PEAK PERIODS.   |                         |                |                |  |
| <div>716</div>  | THE TRAFFIC CONTROL PLAN IS NOT ALL INCLUSIVE. THE TCP PROVIDES SEVERAL DETAILED DRAWINGS INDICATING THE TRAFFIC CONTROL NECESSARY FOR THE DIFFERENT CONSTRUCTION ACTIVITIES ANTICIPATED FOR THIS PROJECT. THE CONTRACTOR SHALL SELECT THE DETAILED DRAWING THAT BEST FITS THE ACTIVITY TO BE PERFORMED.   |  |  | <div>736</div> | CHANNELIZING DRUMS SHOULD BE PLACED ON 25 FOOT INTERVALS THROUGHOUT ALL TAPERS.   |                         |                |                |  |
| 717   | OMITTED  |  |  | <div>737</div> | CHANNELIZING DEVICES SHALL EXTEND TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.   |                         |                |                |  |
| <div>718</div>  | REQUIRED TEMPORARY ROUTE MARKER ASSEMBLIES THAT ARE TO BE LOCATED IN THE VICINITY OF EXISTING ROUTE MARKERS SHOULD BE PLACED ALONG SIDE OF THOSE ALREADY IN PLACE. SOME EXISTING ROUTE MARKERS MAY HAVE TO BE COVERED OR REMOVED, AS DIRECTED BY THE ENGINEER. COST SHALL BE A SUBSIDIARY OBLIGATION OF ITEM 740B.   |  |  | 738            | OMITTED   |                         |                |                |  |
|   |  |  |  | <div>739</div> | THE QUANTITIES SHOWN IN THE TRAFFIC CONTROL SUMMARY BOX ARE CONSIDERED TO BE THE MINIMUM REQUIREMENT FOR HANDLING TRAFFIC AT ANY GIVEN TIME DURING CONSTRUCTION.  |                         |                | <div>753</div> | THE CONTRACTOR SHALL NOT HAVE LANE CLOSURES MONDAY THROUGH FRIDAY FROM 7AM TO 8:30 AM OR 3 PM TO 6 PM.   |
|   |  |  |  | <div>740</div> | LIGHTWEIGHT TYPE B WARNING LIGHTS (WEIGHING 3.3 POUNDS OR LESS) WITH DETACHABLE HEADS MAY BE USED ON DRUMS IN SPECIAL SITUATIONS AS SHOWN ON THE PLANS. TYPE B WARNING LIGHTS WITH DETACHABLE HEADS USED ON BARRICADES SHALL BE LIGHTWEIGHT (WEIGHING 3.3 POUNDS OR LESS). ANY HEAVYWEIGHT WARNING LIGHTS ON BARRICADES MUST BE CERTIFIED BY THE VENDOR AS TO CRASHWORTHINESS OF THE BARRICADE AND WARNING LIGHT COMBINATION. |                         |                |                |  |
|   |  |  |  | 741            | FOR DIVIDED ROADWAYS, THE REQUIRED ADVANCE WARNING SIGNS SHALL BE POSTED ON BOTH THE RIGHT AND LEFT SIDE OF THE ROADWAY.  |                         |                |                |  |
|   |  |  |  | <div>742</div> | THE CONTRACTOR SHALL CLOSE THE LANE ADJACENT TO THE WORK AREA ANYTIME WORK OUTSIDE THE EXISTING TRAVEL LANES ENCROACHES WITHIN 2 FEET OF THE EXISTING EDGE OF PAVEMENT.   |                         |                |                |  |
|   |  |  |  | 743            | OMITTED   |                         |                |                |  |
|   |  |  |  | <div>744</div> | THE TRANSITION TAPER LENGTH (L) IS SHOWN IN TABLE 6C-4, AND THE BUFFER LENGTH IS SHOWN IN TABLE 6C-2 OF THE MUTCD, PART 6, 2009 EDITION.  |                         |                |                |  |
|   |  |  |  | 745            | OMITTED   |                         |                |                |  |
|   |  |  |  | <div>746</div> | UNEVEN LANES SIGNS SHALL BE COVERED OR REMOVED WHEN NO UNEVEN PAVEMENT CONDITIONS EXIST.  |                         |                |                |  |
|   |  |  |  | <div>747</div> | MOVING OPERATIONS SHALL BE CONFINED TO ONE LANE IN THE DIRECTION OF TRAFFIC.  |                         |                |                |  |
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| SUMMARY OF QUANTITIES |  |  |  |  |  |  | REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
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## SUMMARY OF QUANTITIES

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 3-A         |

| REQUIRED DRAINAGE STRUCTURE REMOVAL |               |             |      |            |                       |                      |                    |
|-------------------------------------|---------------|-------------|------|------------|-----------------------|----------------------|--------------------|
| ROADWAY                             | BEGIN STATION | END STATION | SIDE | PLAN SHEET | DESCRIPTION           | REMOVING PIPE        | REMOVING HEADWALLS |
|                                     |               |             |      |            |                       | 206D-000<br>(LIN FT) | 206E-000<br>(EACH) |
| CR-17                               | 16+10.38      | 16+25.95    | RT   | 4          | 36" RCP (TRIPLE LINE) | 10                   | 1                  |
| TOTAL                               |               |             |      |            |                       | 10                   | 1                  |

| REQUIRED DRAINAGE QUANTITIES |          |    |          |      |                |                |              |            |                |   |   |                      |                     |                          |                                |                     |                  |          |          |          |
|------------------------------|----------|----|----------|------|----------------|----------------|--------------|------------|----------------|---|---|----------------------|---------------------|--------------------------|--------------------------------|---------------------|------------------|----------|----------|----------|
| ROADWAY                      | STATION  | TO | STATION  | SIDE | SKEW           | FROM INDEX NO. | TO INDEX NO. | PLAN SHEET | DRAINAGE SHEET | ROADWAY PIPE (CLASS 3 R.C.) (EXTENSION) | ROADWAY PIPE END TREATMENT, CLASS 2 (TRIPLE LINE) | STRUCTURE EXCAVATION | FOUNDATION BACKFILL | MINOR STRUCTURE CONCRETE | PIPE END TREATMENT SLOPE (X:1) | MAXIMUM FILL HEIGHT | STD OR SPEC DWG  |          |          |          |
|                              |          |    |          |      |                |                |              |            |                | 530A-104                                | 619A-259  |                      |                     |                          |                                |                     |                  |          |          |          |
|                              |          |    |          |      |                |                |              |            |                | 36"                                     | 36"   |                      |                     |                          |                                |                     |                  | 214A-000 | 214B-000 | 620A-000 |
|                              |          |    |          |      |                |                |              |            |                | (LIN FT)                                | (EACH)  |                      |                     |                          |                                |                     |                  | (CU YD)  | (CU YD)  | (CU YD)  |
| CR-17                        | 16+10.38 | TO | 16+25.95 | RT   | 0°35'09.67" LA | 1              | 2            | 4          | 41             | 25                                      | 1   | 9                    | 3                   | 2                        | 3                              | 0.7                 | CC-530, HW-614-B |          |          |          |
|                              |          |    |          |      |                |                |              |            |                |   |   |                      |                     |                          |                                |                     |                  |          |          |          |
| TOTAL                        |          |    |          |      |                |                |              |            |                | 25                                      | 1   | 9                    | 3                   | 2                        |                                |                     |                  |          |          |          |

| REQUIRED RIGHT OF WAY MARKERS |          |           |      |                         |                    |             |
|-------------------------------|----------|-----------|------|-------------------------|--------------------|-------------|
| ROADWAY                       | STATION  | OFFSET    | SIDE | RIGHT OF WAY<br>MARKERS | STD OR SPEC<br>DWG | REMARKS     |
|                               |          |           |      | 602A-000                |                    |             |
|                               |          |           |      | EACH                    |                    |             |
| CR-17                         | 13+64.41 | +/- 30.80 | RT   | 1                       | L                  | Tie to PRES |
| CR-17                         | 15+25.53 | +/- 31.96 | RT   | 1                       | L                  | Tie to PRES |
|                               |          |           |      |                         |                    |             |
| TOTAL                         |          |           |      | 2                       |                    |             |

|  |                                      |                                    |                             |                |  |              |                       |                  |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|--|--------------|-----------------------|------------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  | NOT TO SCALE | SHEET TITLE           | ROUTE            |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |  |              | SUMMARY OF QUANTITIES | CR-17 @<br>CR-44 |

SUMMARY OF QUANTITIES

REFERENCE  
PROJECT NO

FISCAL  
YEAR

SHEET  
NO

SCP 59-889-18

2023

3-B

| REQUIRED EROSION AND SEDIMENT CONTROL |                  |                |                                  |          |          |                               |          |                      |                       |              |                                 |               |  |                       |              |                              |                    |  |
|---------------------------------------|------------------|----------------|----------------------------------|----------|----------|-------------------------------|----------|----------------------|-----------------------|--------------|---------------------------------|---------------|--|-----------------------|--------------|------------------------------|--------------------|--|
| ROADWAY                               | BEGIN<br>STATION | END<br>STATION | FILTER<br>BLANKET,<br>GEOTEXTILE | TOPSOIL  | SEEDING  | SOLID<br>SODDING<br>(BERMUDA) | MULCHING | TEMPORARY<br>SEEDING | TEMPORARY<br>MULCHING | POLYETHYLENE | TEMPORARY<br>RIPRAP,<br>CLASS 2 | SILT<br>FENCE | TEMPORARY<br>COARSE<br>AGGREGATE,<br>ALDOT<br>NUMBER 1 | SILT FENCE<br>REMOVAL | WATTLE       | PEST<br>CONTROL<br>TREATMENT | STD OR<br>SPEC DWG | REMARKS                                    |
|                                       |                  |                | 610D-003                         | 650A-000 | 652A-100 | 654A-001                      | 656A-010 | 665A-000             | 665B-001              | 665E-000     | 665I-000                        | 665J-002      | 665N-000   | 665O-001              | 665Q-00<br>2 | 666A-001                     |                    |  |
|                                       |                  |                | (SQ YD)                          | (CU YD)  | (ACRE)   | (SQ YD)                       | (ACRE)   | (ACRE)               | (TON)                 | (SQ YD)      | (TON)                           | (LIN FT)      | (TONS)   | (LIN FT)              | (LIN FT)     | (ACRE)                       |                    |  |
| CR-17                                 | 11+18.10         | 17+32.30       | 52                               | 133      | 0.16     | 796                           | 0.16     | 0.16                 | 0.16                  | 500          | 25                              | 455           | 25   | 455                   | 48           | 0.16                         |                    | RIPRAP TO BE USED FOR<br>ROCK DITCH CHECKS |
| TOTAL                                 |                  |                | 52                               | 133      | 1        | 796                           | 1        | 1                    | 1                     | 500          | 25                              | 455           | 25   | 455                   | 48           | 1                            |                    |  |

| REQUIRED STRIPING, PAVEMENT MARKERS, MARKINGS AND LEGENDS |                  |                |   |   |                                   |   |  |  |  |  |                 |               |  |
|---|------------------|----------------|---|---|-----------------------------------|---|--|--|--|--|-----------------|---------------|--|
| ROADWAY   | BEGIN<br>STATION | END<br>STATION | DOTTED, CLASS 2, TYPE A<br>TRAFFIC STRIPE (5" WIDE) | DOTTED, CLASS 2, TYPE A<br>TRAFFIC STRIPE (5" WIDE) | SOLID TEMPORARY<br>TRAFFIC STRIPE | TRAFFIC CONTROL<br>MARKINGS,<br>CLASS 2, TYPE A | TRAFFIC CONTROL<br>LEGENDS,<br>CLASS 2, TYPE A | TEMPORARY TRAFFIC<br>CONTROL MARKINGS<br>(PAINT) | PAVEMENT MARKERS,<br>CLASS A-H, TYPE 2-C | PAVEMENT MARKERS,<br>CLASS A-H, TYPE 1-A | STD OR SPEC DWG | PROJECT NOTES |  |
|   |                  |                | 701B-   | 701G-   | 701E-                             | 703A-   | 703B-  | 703D-  | 705A-                                    |  |                 |               |  |
|   |                  |                | 207   | 253   | 000                               | 002   | 002  | 002  | 030                                      | 031                                      |                 |               |  |
|   |                  |                | (LIN FT)  | (LIN FT)  | (LIN FT)                          | (SQ FT)   | (SQ FT)  | (SQ FT)  | (EACH)                                   | (EACH)                                   |                 |               |  |
| CR-17   | 11+18.10         | 17+32.30       | 100   | 1147  | 1147                              | 266   | 45   | 266  | 2  | 7  | A,B,D           | 300           |  |
| TOTAL   |                  |                | 100   | 1147  | 1147                              | 266   | 45   | 266  | 2  | 7  |                 |               |  |

| REQUIRED GROUND MOUNTED SIGNS |                        |          |                |      |  |                 |      |   |                     |
|-------------------------------|------------------------|----------|----------------|------|--|-----------------|------|---|---------------------|
| SIGN ID NUMBER                | SIGNING PLAN SHEET NO. | STATION  | ROADWAY        | SIDE | ROADWAY SIGN<br>POST (#3<br>U-CHANNEL,<br>GALVANIZED STEEL<br>OR 2", 14 GA<br>SQUARE TUBULAR<br>STEEL) | NO. OF<br>POSTS | R1-2 | CLASS 8,<br>ALUMINUM<br>FLAT SIGN<br>PANELS 0.08"<br>THICK OR<br>STEEL FLAT<br>SIGN PANELS<br>14 GAUGE<br>(TYPE IX<br>BACKGROUND) | STD OR SPEC<br>DWGS |
|                               |                        |          |                |      | 710B-021   |                 |      | 710A-126  |                     |
|                               |                        |          |                |      | (LIN FT)   |                 |      | SQ FT   |                     |
| 1                             | 6                      | 10+64.40 | COUNTY ROAD 44 | LT   | 14   | 1               | 1    | 3.90  | A                   |
| TOTAL                         |                        |          |                |      | 14   |                 |      | 4   |                     |

RESPONSIBLE PE: James R. Brown, P.E.

SUPERVISOR: Doug R. Peterson, P.E.

DESIGNER: Dayla Baugh, P.E.

PLAN SUBMITTAL

DATE:

DATE:

DATE:

PERMIT

NOT TO SCALE

SHEET TITLE

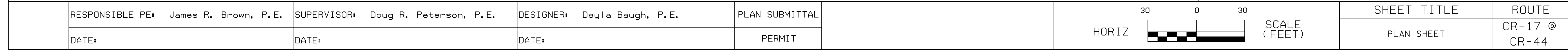
ROUTE

SUMMARY OF QUANTITIES

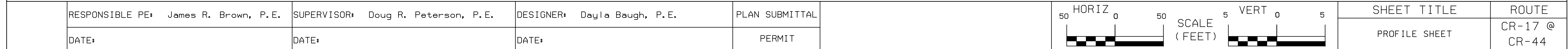
CR-17 @  
CR-44



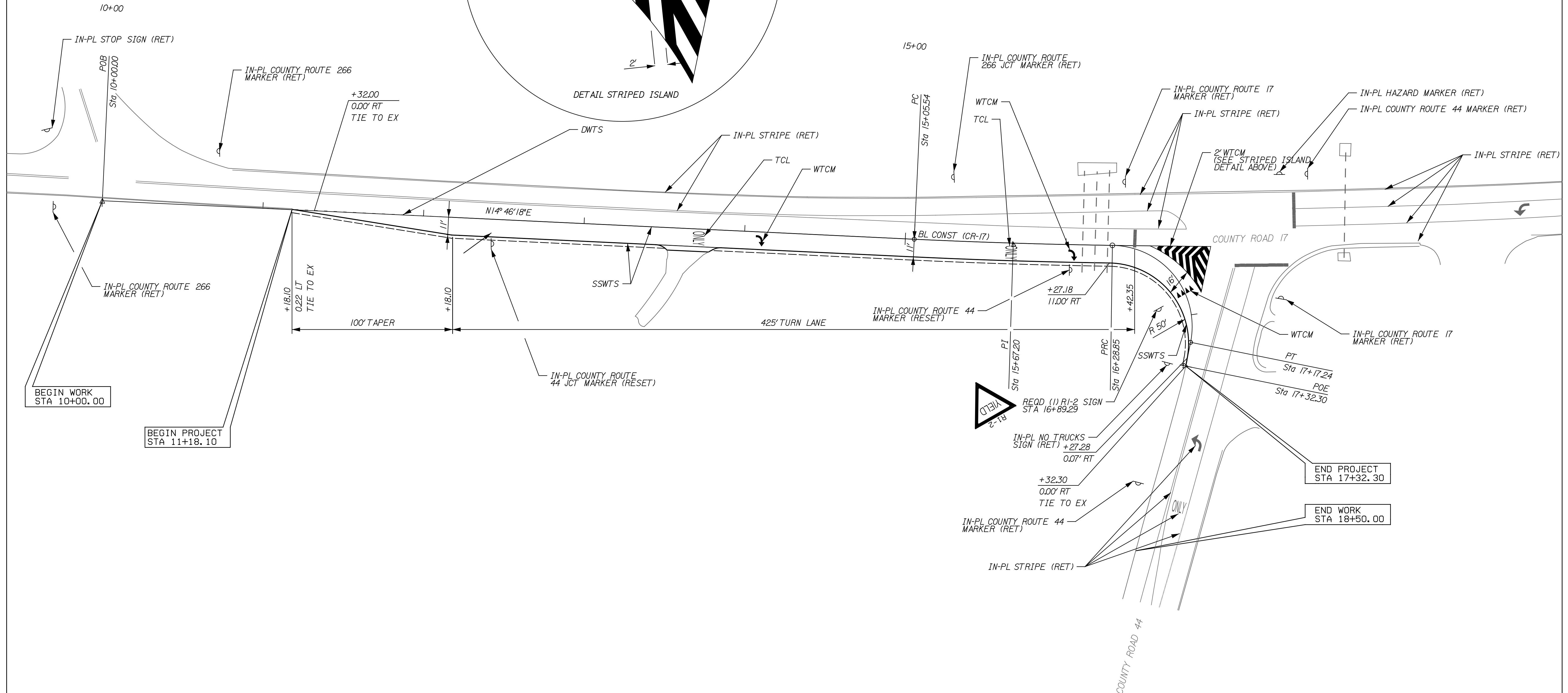
| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 4           |




| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 4-A         |



| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 6           |

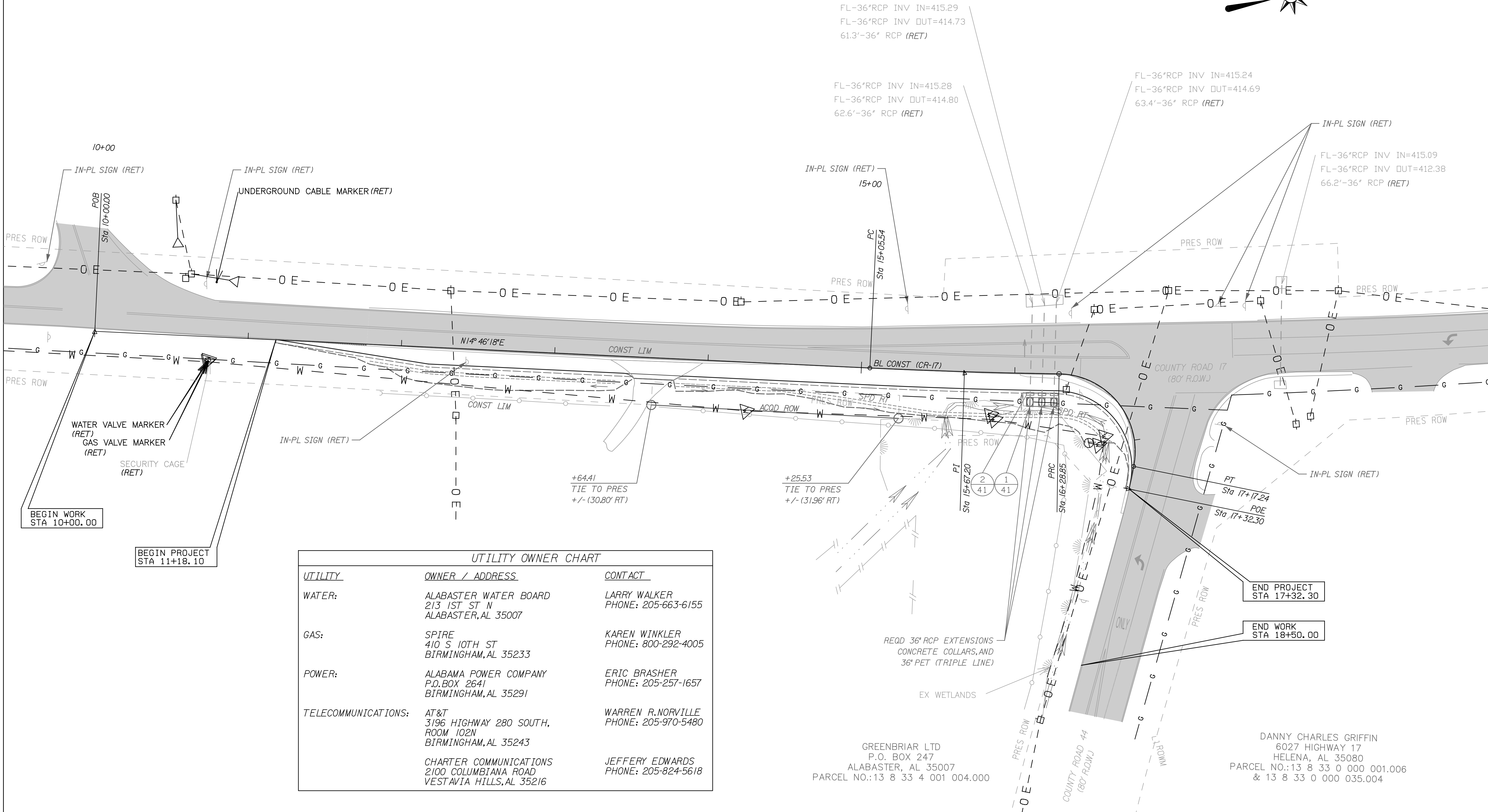
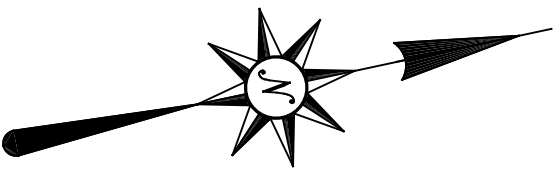


|  |                                      |                                    |                             |                |   |   |               |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|---|---|---------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | <div><div>HORIZ</div><div><div>30030</div><div></div></div><div>SCALE (FEET)</div></div> | SHEET TITLE                               | ROUTE         |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |   | PAVING LAYOUT, SIGNING AND STRIPING SHEET | CR-17 @ CR-44 |

UTILITY PLAN SHEET

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 8           |

PROJECT NOTES  
800



| UTILITY OWNER CHART |  |   |
|---------------------|--|---|
| UTILITY             | OWNER / ADDRESS  | CONTACT                                   |
| WATER:              | ALABASTER WATER BOARD<br>213 1ST ST N<br>ALABASTER, AL 35007               | LARRY WALKER<br>PHONE: 205-663-6155       |
| GAS:                | SPIRE<br>410 S 10TH ST<br>BIRMINGHAM, AL 35233                             | KAREN WINKLER<br>PHONE: 800-292-4005      |
| POWER:              | ALABAMA POWER COMPANY<br>P.O. BOX 2641<br>BIRMINGHAM, AL 35291             | ERIC BRASHER<br>PHONE: 205-257-1657       |
| TELECOMMUNICATIONS: | AT&T<br>3196 HIGHWAY 280 SOUTH,<br>ROOM 102N<br>BIRMINGHAM, AL 35243       | WARREN R. NORVILLE<br>PHONE: 205-970-5480 |
|                     | CHARTER COMMUNICATIONS<br>2100 COLUMBIANA ROAD<br>VESTAVIA HILLS, AL 35216 | JEFFERY EDWARDS<br>PHONE: 205-824-5618    |

|                                      |                                    |                             |                |       |              |                    |                  |
|--------------------------------------|------------------------------------|-----------------------------|----------------|-------|--------------|--------------------|------------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | HORIZ | SCALE (FEET) | SHEET TITLE        | ROUTE            |
|                                      |                                    |                             |                |       |              |                    |                  |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         |       |              | UTILITY PLAN SHEET | CR-17 @<br>CR-44 |

TRAFFIC CONTROL SEQUENCE OF CONSTRUCTION AND QUANTITIES

NOTES:

- ANY WORK CAN BE PERFORMED CONCURRENTLY, WITH THE APPROVAL OF THE ENGINEER, AS LONG AS IT DOES NOT CONFLICT WITH ANOTHER SEQUENCED ITEM OF WORK.

SEQUENCE OF CONSTRUCTION

PHASE I:

- INSTALL ADVANCED WARNING SIGNS AND APPLICABLE TRAFFIC CONTROL DEVICES.

PHASE II:

- UTILIZE THE TRAFFIC CONTROL STANDARD DETAILS TO CONSTRUCT THE GRADING, DRAINAGE AND PAVING OPERATION FOR THE REQUIRED RIGHT TURN LANE THROUGH THE WEARING SURFACE.
- PLACE TEMPORARY STRIPE IN THE PERMANENT LOCATIONS.
- CONSTRUCT TEMPORARY TRAFFIC SIGNAL.

PHASE III:

- INSTALL THE REQUIRED PERMANENT STRIPING, MARKINGS, SIGNING, AND MARKERS AS SHOWN ON THE SIGNING AND STRIPING LAYOUT SHEETS.
- COMPLETE ANY REMAINING ITEMS OF WORK.
- REMOVE ANY REMAINING TRAFFIC CONTROL DEVICES.

CONSTRUCTION SIGNS (740B-000)

| SIGN NO    | DESCRIPTION         | SIZE    | QUANTITY | AREA (SQ FT) | TOTAL AREA (SQ FT) | STD DWG NO | SIGN MOUNT |
|------------|---------------------|---------|----------|--------------|--------------------|------------|------------|
| G20-2      | END ROAD WORK       | 48"X24" | 2        | 8.00         | 16                 | SHS-30     | P          |
| R2-1       | SPEED LIMIT 30 MPH  | 24"X30" | 2        | 5.00         | 10                 | SHS-1      | P          |
| R2-1       | SPEED LIMIT 35 MPH  | 24"X30" | 2        | 5.00         | 10                 | SHS-1      | P          |
| R2-1       | SPEED LIMIT 40 MPH  | 24"X30" | 2        | 5.00         | 10                 | SHS-1      | P          |
| R5-1       | DO NOT ENTER        | 48"X30" | 2        | 6.25         | 12.5               | SHS-3      | B          |
| R11-2      | ROAD CLOSED         | 48"X30" | 2        | 10.00        | 20                 | SHS-8      | B          |
| W3-5b      | REDUCED SPEED AHEAD | 48"X48" | 2        | 16.00        | 32                 | SHS-26     | P          |
| W20-1      | ROAD WORK AHEAD     | 48"X48" | 2        | 16.00        | 32                 | SHS-29     | P          |
| W20-1      | ROAD WORK 500 FT    | 48"X48" | 2        | 16.00        | 32                 | SHS-29     | P          |
| W20-1      | ROAD WORK 1000 FT   | 48"X48" | 2        | 16.00        | 32                 | SHS-29     | P          |
| W20-1      | ROAD WORK 1500 FT   | 48"X48" | 2        | 16.00        | 32                 | SHS-29     | P          |
| W20-4      | ONE LANE AHEAD      | 36"X36" | 2        | 9.00         | 18                 | SHS-29     | P          |
| W20-7      | FLAGGER             | 36"X36" | 2        | 9.00         | 18                 | SHS-29     | P          |
| TOTAL AREA |                     |         |          |              | 275                |            |            |

LEGEND:  
P: POST  
B: BARRICADE  
T: TEMPORARY

|          |                        |       |     |   |
|----------|------------------------|-------|-----|---|
| 740B-000 | CONSTRUCTION SIGNS     | SQ FT | 275 | TCD-100, IHS-710-12, IHS-710-21, IHS-710-23 |
| 740D-000 | CHANNELIZING DRUMS     | EACH  | 40  | TCD-100                                     |
| 740E-000 | CONES (36 INCHES HIGH) | EACH  | 50  | TCD-100                                     |
| 740F-002 | BARRICADES, TYPE III   | EACH  | 4   | B-107-2                                     |
| 740I-002 | WARNING LIGHTS, TYPE B | EACH  | 2   | B-107-2                                     |
| 740M-001 | BALLAST FOR CONES      | EACH  | 50  | TCD-100                                     |

RESPONSIBLE PE: James R. Brown, P.E.

DATE:

SUPERVISOR: Doug R. Peterson, P.E.

DATE:

DESIGNER: Dayla Baugh, P.E.

DATE:

PLAN SUBMITTAL

PERMIT

NOT TO SCALE

TRAFFIC CONTROL SEQUENCE OF CONSTRUCTION AND QUANTITIES

ROUTE

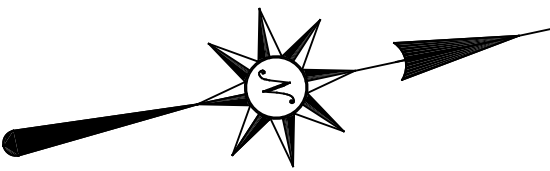
CR-17 @ CR-44



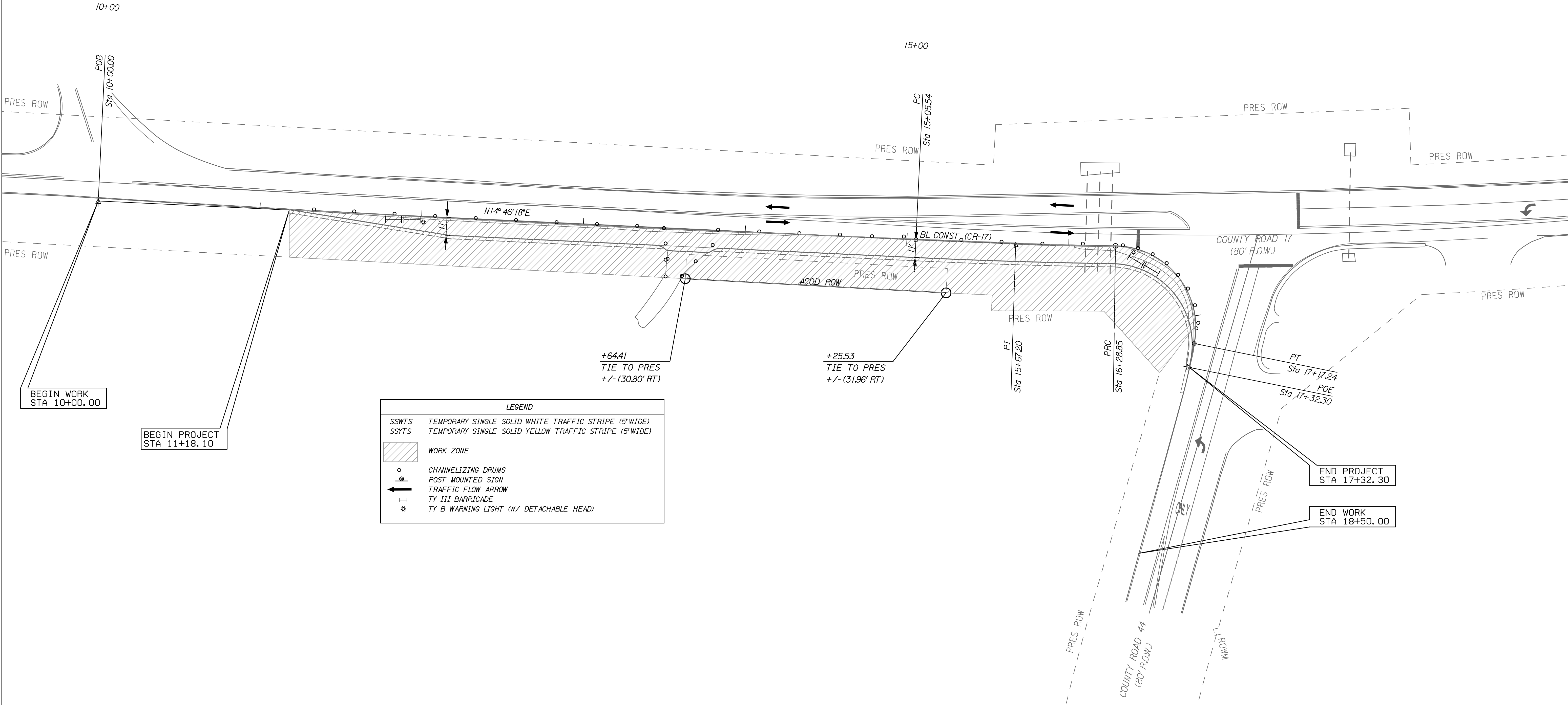
TRAFFIC CONTROL PLAN

PHASE II

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 11          |



TRAFFIC CONTROL NOTES  
700,701,702,703,704,705,707,  
708,710,711,712,713,715,716,  
718,720,725,727,729,732,733,  
734,735,736,737,739,740,742,  
744,746,747,749,751,752,753



| LEGEND |  |
|--------|--|
| SSWTS  | TEMPORARY SINGLE SOLID WHITE TRAFFIC STRIPE (5' WIDE)  |
| SSYTS  | TEMPORARY SINGLE SOLID YELLOW TRAFFIC STRIPE (5' WIDE) |
|        | WORK ZONE  |
|        | CHANNELIZING DRUMS                                     |
|        | POST MOUNTED SIGN                                      |
|        | TRAFFIC FLOW ARROW                                     |
|        | TY III BARRICADE                                       |
|        | TY B WARNING LIGHT (W/ DETACHABLE HEAD)                |

|  |                                      |                                    |                             |                |   |                      |                  |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|---|----------------------|------------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | <div>HORIZ</div> <div>50 0 50</div> <div>SCALE (FEET)</div> | SHEET TITLE          | ROUTE            |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |   | TRAFFIC CONTROL PLAN | CR-17 @<br>CR-44 |

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 12          |



NOTE: THE CONTRACTOR IS TO CONSTRUCT A WEDGE OF UNCLASSIFIED EXCAVATION OR CRUSHED AGGREGATE BASE. THE COST OF PLACING AND REMOVAL SHALL BE A SUBSIDIARY OBLIGATION OF THE ADJACENT LAYER.

Diagram illustrating the plan view of a 30' wide equipment parking area. The area is defined by a solid line labeled "EDGE OF PAVEMENT" and a dashed line labeled "OR" with arrows indicating traffic flow. The parking area is marked with "REFLECTORIZED CHANNELIZING DRUMS" along its perimeter. The distance from the "BEGIN TAPER" to the first drum is 30'. The distance between the first and second drums is 25'. The total length of the taper is 100'. The distance between the last drum and the "EDGE OF PAVEMENT" is 50'. The parking area is labeled "EQUIPMENT PARKING AREA". A note "SEE NOTE NO 2" points to the drums along the equipment parking area.


1. SEE ALDOT'S GENERAL TRAFFIC CONTROL PLAN NOTE NO. 702.
2. DRUMS TO BE AS FAR AS PRACTICAL FROM EDGE OF PAVEMENT, MINIMUM DESIRABLE DISTANCE IS 15 FEET FOR FREEWAY TYPE FACILITIES AND 10 FEET FOR OTHER FACILITIES. FOR UNUSUAL CONDITIONS, SUCH AS SPECIAL EQUIPMENT OR LIMITED AVAILABLE SPACE, DIMENSIONS LESS THAN DESIRABLE SHALL BE AS DIRECTED BY THE ENGINEER.
3. ALL DEVICES TO BE FURNISHED BY THE CONTRACTOR WITHOUT COST TO THE ALDOT.

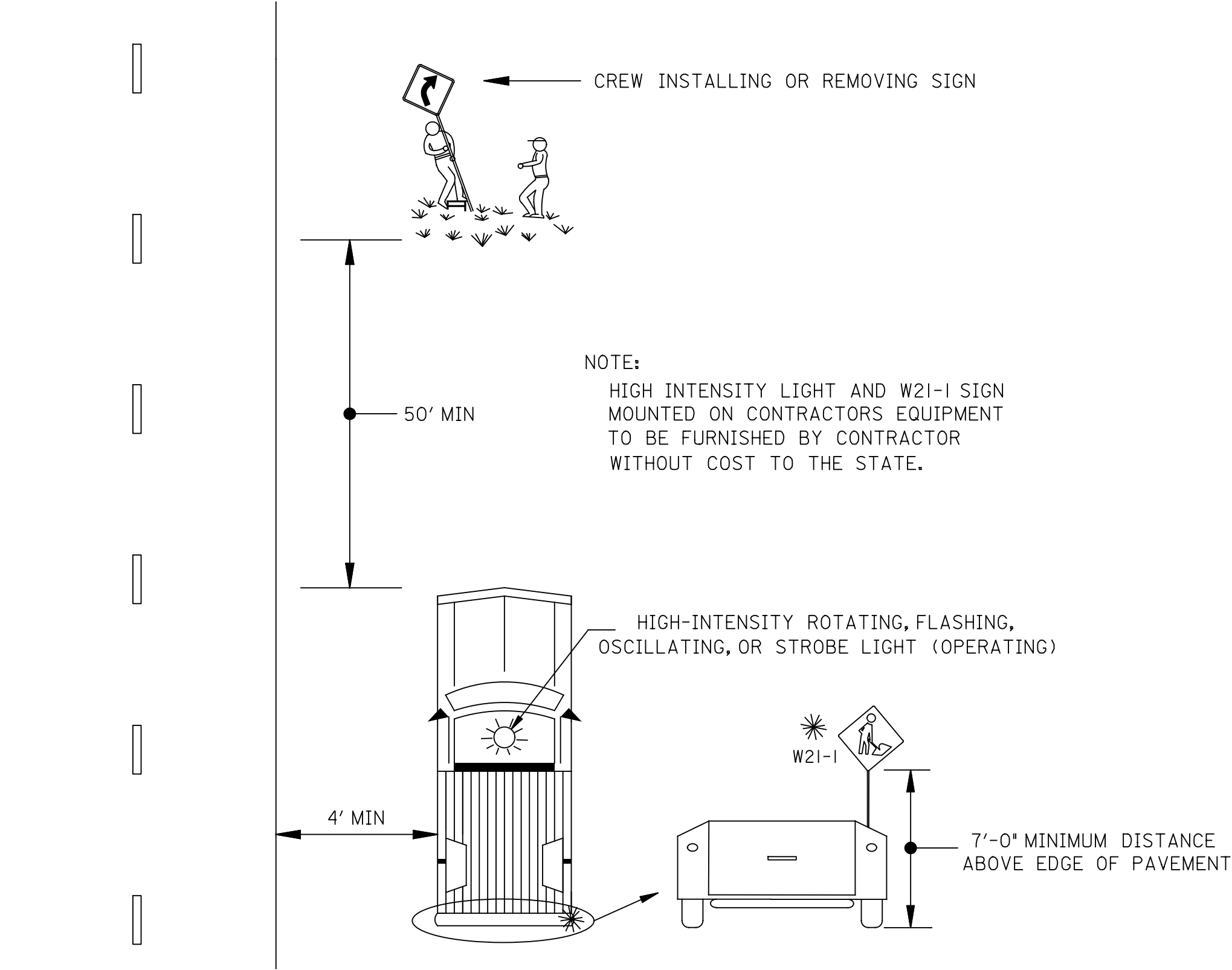
### DETAILS FOR TYPICAL PLACEMENT OF TYPE III BARRICADES INSIDE OF CLEAR ZONE



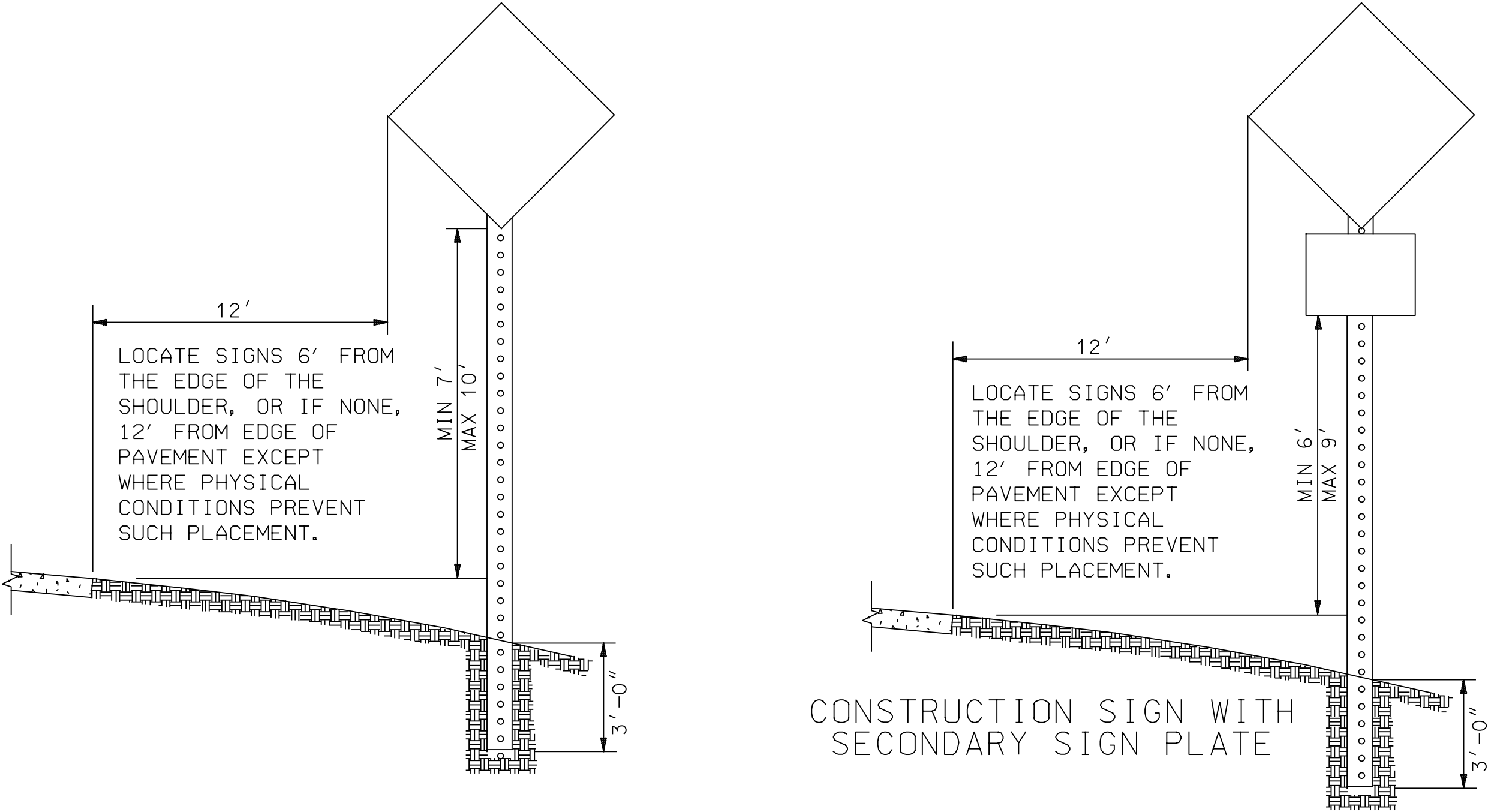
1. SLOPE OF STRIPES ON BARRICADES SHALL BE IN ACCORDANCE WITH SECTION 6F.68 OF THE MUTCD AND DRAWING B-107-2.
2. IF SIGNS ARE REQUIRED TO BE USED IN CONJUNCTION WITH TYPE III BARRICADES TO BE PLACED INSIDE THE CLEAR ZONE, THEY SHALL BE POST MOUNTED TO THE SIDE OF THE BARRICADES AS SHOWN.
3. IF ROAD CLOSED OR DETOUR SIGNS ARE REQUIRED TO BE USED WITH TYPE III BARRICADES TO BE PLACED OUTSIDE THE CLEAR ZONE, THEY SHALL BE PLACED ON THE TOP OF THE BARRICADES NEAREST THE DETOUR.

|  |                                      |                                    |                             |                |              |                      |                  |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|--------------|----------------------|------------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | NOT TO SCALE | SHEET TITLE          | ROUTE            |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |              | TRAFFIC CONTROL PLAN | CR-17 @<br>CR-44 |

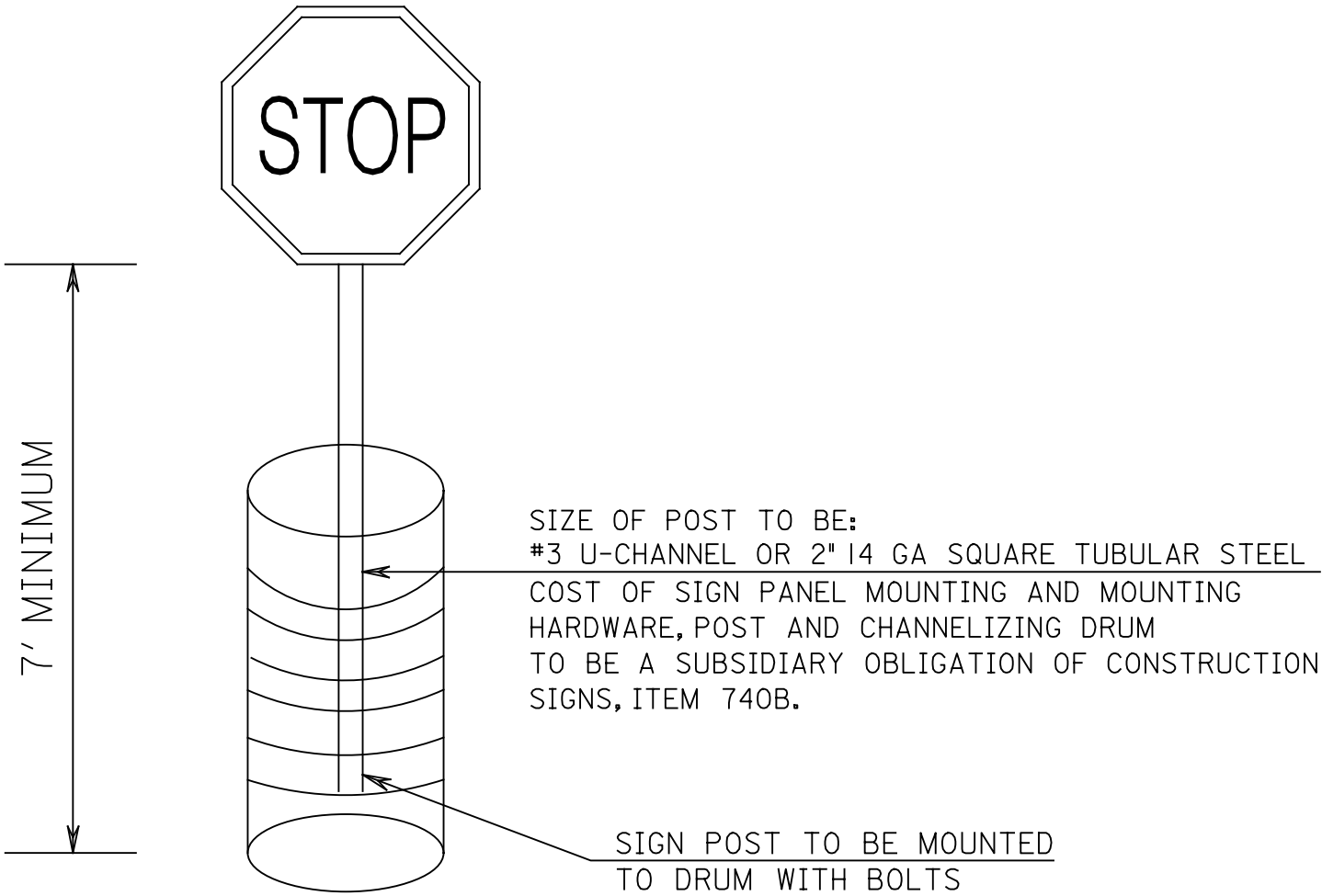
|   |  |   |  |
|---|--|---|--|
| <p align="center">--SPECIFICATIONS--</p> <p align="center">CURRENT ALABAMA DEPARTMENT OF TRANSPORTATION</p>   |  |   |  |
| <p>THIS DRAWING REPRESENTS DESIGNS PREPARED FOR USE BY THE ALABAMA DEPARTMENT OF TRANSPORTATION AND IS NOT TO BE COPIED, REPRODUCED, ALTERED, OR USED BY ANYONE, OR ANY ORGANIZATION, WITHOUT THE EXPRESSED WRITTEN CONSENT OF THE ALABAMA DEPARTMENT OF TRANSPORTATION REPRESENTATIVE AUTHORIZED TO APPROVE THIS USE. ANYONE MAKING UNAUTHORIZED USE OF THIS DRAWING MAY BE PROSECUTED TO THE FULLEST EXTENT OF THE LAW.</p> |  |   |  |
| <p><u>REVISIONS</u></p>   |  | <div style="display: flex; align-items: center;">  <div> <p align="center"><b>ALABAMA DEPARTMENT<br/>OF TRANSPORTATION</b></p> <p align="center">1409 COLISEUM BOULEVARD<br/>MONTGOMERY, AL 36130-3050</p> </div> </div> |  |
|   |  | <p align="center">DESIGN BUREAU SPECIAL DRAWING</p> <p align="center"><b>STANDARD DETAILS FOR<br/>TRAFFIC CONTROL PLANS</b></p>   |  |
| <p>DRAWN BY: _____</p> <p>DATE DRAWN: <u>9/8/2014</u></p>   |  | <p align="center">SPECIAL DRAWING NO.</p> <p align="center"><b>SPECIAL PROJECT DETAIL</b></p>   |  |
|   |  | <p align="right">INDEX NO.<br/><b>2001</b></p>  |  |



TYPICAL METHOD FOR INSTALLING OR REMOVING CONSTRUCTION SIGNS



HEIGHT AND LATERAL LOCATION OF POST MOUNTED CONSTRUCTION SIGNS



DETAIL FOR DRUM MOUNTED CONSTRUCTION SIGNS

THIS DRAWING REPRESENTS DESIGNS PREPARED FOR USE BY THE ALABAMA DEPARTMENT OF TRANSPORTATION AND IS NOT TO BE COPIED, REPRODUCED, ALTERED, OR USED BY ANYONE, OR ANY ORGANIZATION, WITHOUT THE EXPRESSED WRITTEN CONSENT OF THE ALABAMA DEPARTMENT OF TRANSPORTATION REPRESENTATIVE AUTHORIZED TO APPROVE THIS USE. ANYONE MAKING UNAUTHORIZED USE OF THIS DRAWING MAY BE PROSECUTED TO THE FULLEST EXTENT OF THE LAW.

**ALABAMA DEPARTMENT OF TRANSPORTATION**  
1409 COLISEUM BOULEVARD  
MONTGOMERY, AL 36130-3050

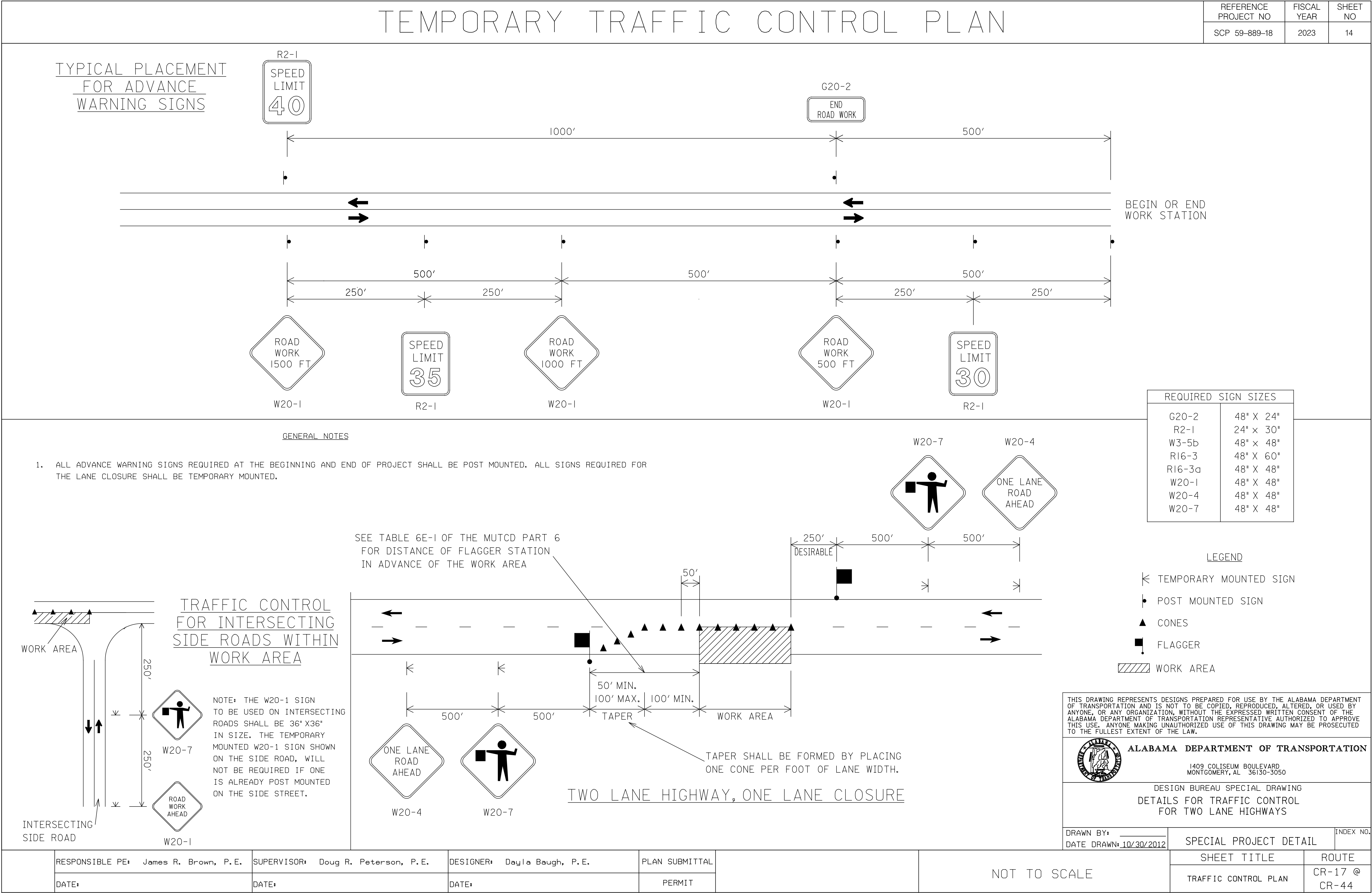
DESIGN BUREAU SPECIAL DRAWING  
STANDARD DETAILS  
FOR TRAFFIC CONTROL PLANS

DRAWN BY: \_\_\_\_\_  
DATE DRAWN: 9/8/2014

SPECIAL PROJECT DETAIL

INDEX NO.  
2001A

|  |                                       |                                     |                              |                |              |                                     |                           |
|--|---------------------------------------|-------------------------------------|------------------------------|----------------|--------------|-------------------------------------|---------------------------|
|  | RESPONSIBLE PE: James R. Brown, P. E. | SUPERVISOR: Doug R. Peterson, P. E. | DESIGNER: Dayla Baugh, P. E. | PLAN SUBMITTAL | NOT TO SCALE | SHEET TITLE<br>TRAFFIC CONTROL PLAN | ROUTE<br>CR-17 @<br>CR-44 |
|  | DATE:                                 | DATE:                               | DATE:                        | PERMIT         |              |                                     |                           |

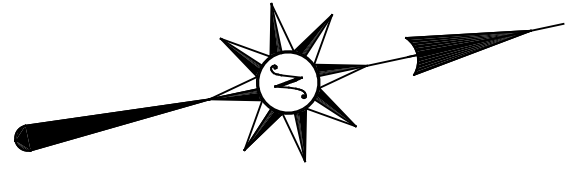


| EROSION AND SEDIMENT CONTROL LEGEND |  |  |  |  |  |  | REFERENCE PROJECT NO | FISCAL YEAR | SHEET NO |
|-------------------------------------|--|--|--|--|--|--|----------------------|-------------|----------|
|                                     |  |  |  |  |  |  | SCP 59-889-18        | 2023        | 30       |
| BEST MANAGEMENT PRACTICES (BMP's)   |  |  |  |  |  |  |                      |             |          |
|                                     |  |  |  |  |  |  |                      |             |          |
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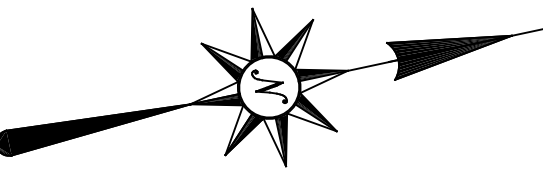
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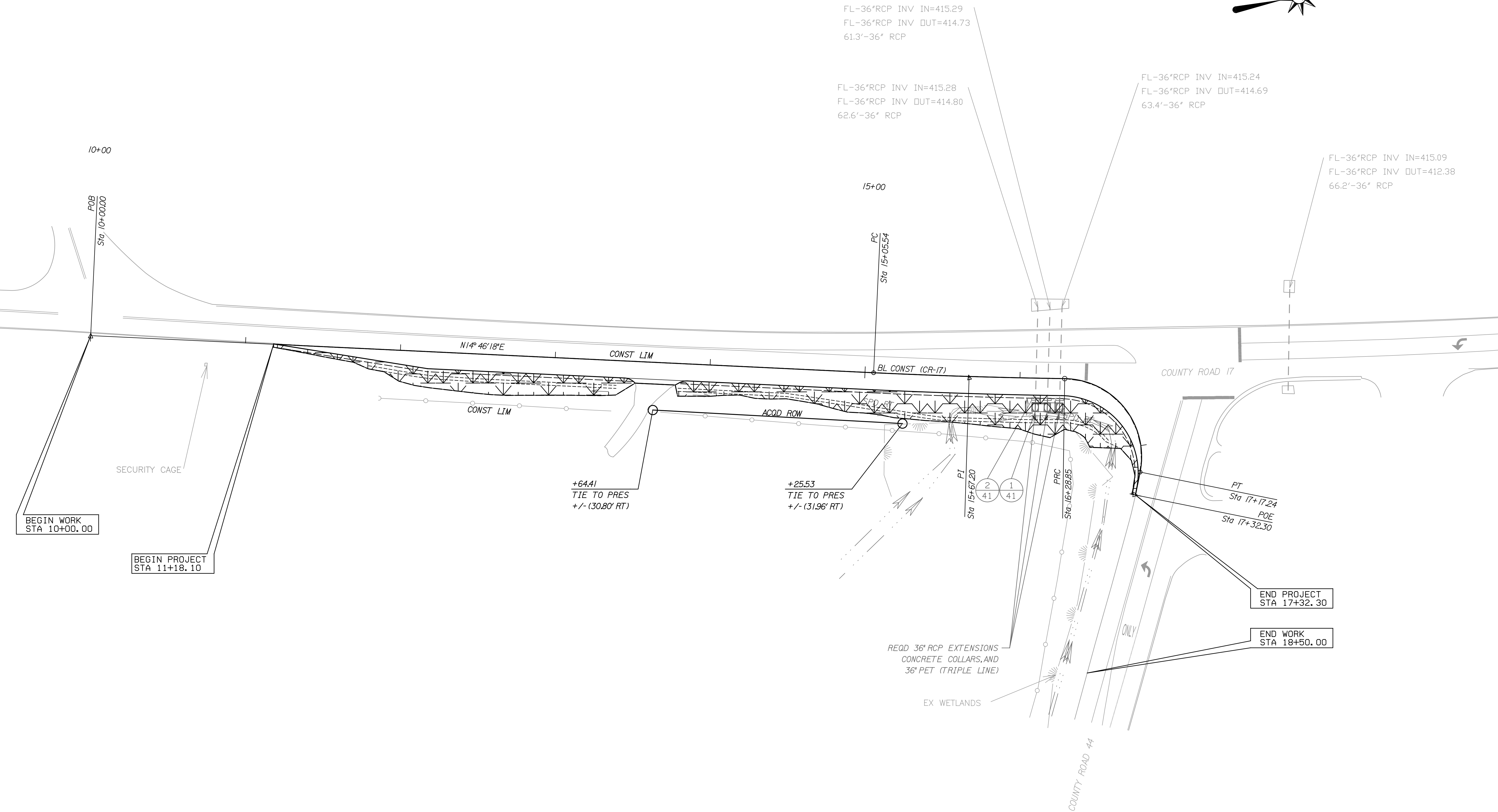
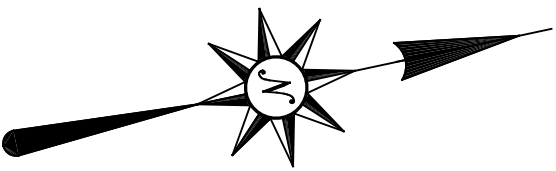
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EROSION AND SEDIMENT CONTROL PLAN SHEET

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 33          |

FINAL CONSTRUCTION

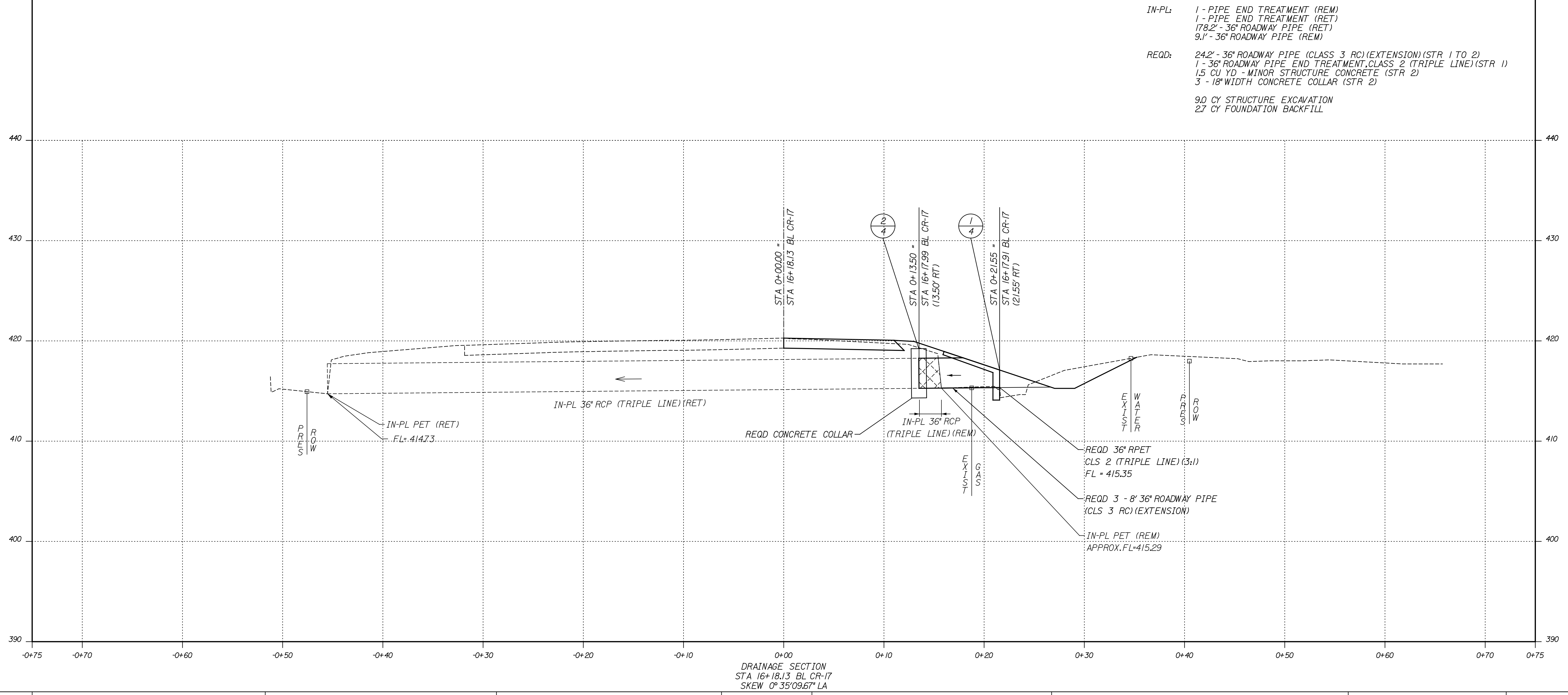
PROJECT NOTES  
900,901



|                                      |                                    |                             |                |  |  |   |  |               |
|--------------------------------------|------------------------------------|-----------------------------|----------------|--|--|---|--|---------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  |  | 30 0 30<br>HORIZ  SCALE (FEET) | SHEET TITLE  | ROUTE         |
|                                      |                                    |                             |                |  |  |   | EROSION AND SEDIMENT CONTROL PLAN SHEET FINAL CONSTRUCTION | CR-17 @ CR-44 |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         |  |  |   |  |               |

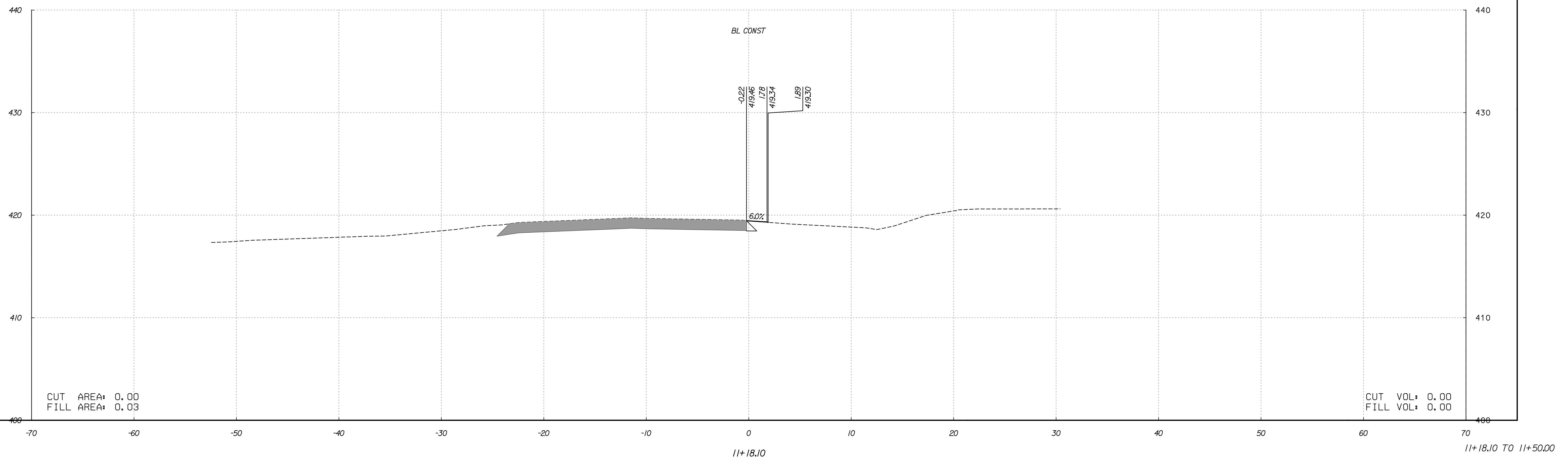
# DRAINAGE CROSS SECTION SHEET

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 41          |



|                                      |                                    |                             |                |                                       |  |                           |
|--------------------------------------|------------------------------------|-----------------------------|----------------|---------------------------------------|--|---------------------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | HORIZ 0 5<br>SCALE (FEET)<br>VERT 0 5 | SHEET TITLE<br>DRAINAGE CROSS<br>SECTION SHEET | ROUTE<br>CR-17 @<br>CR-44 |
|                                      |                                    |                             |                |                                       |  |                           |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         |                                       |  |                           |

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 50          |

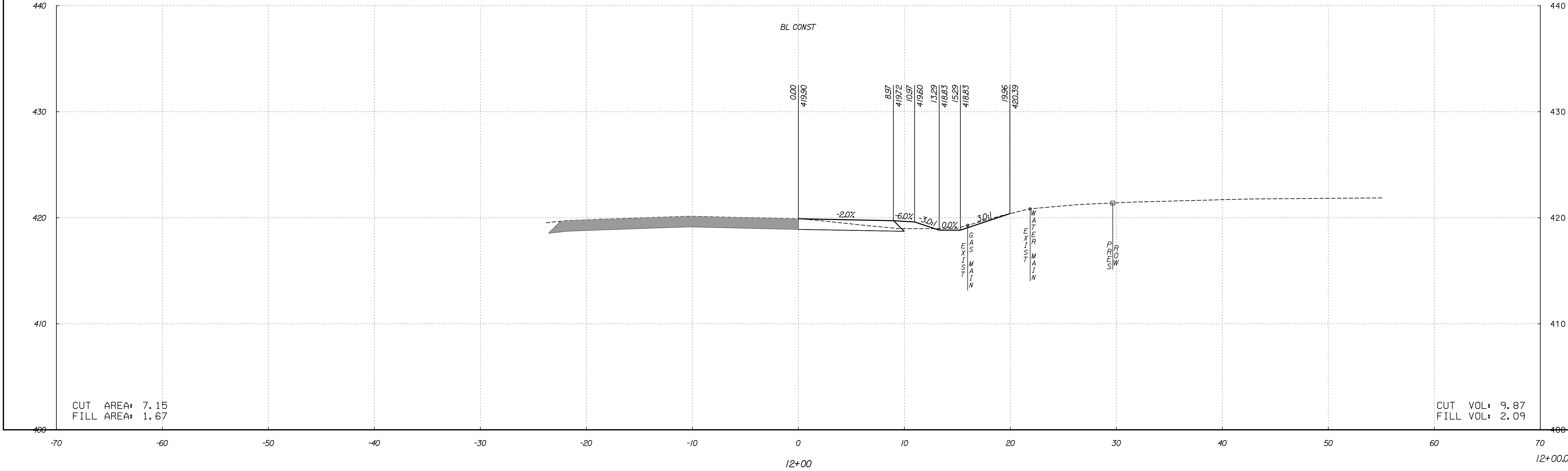
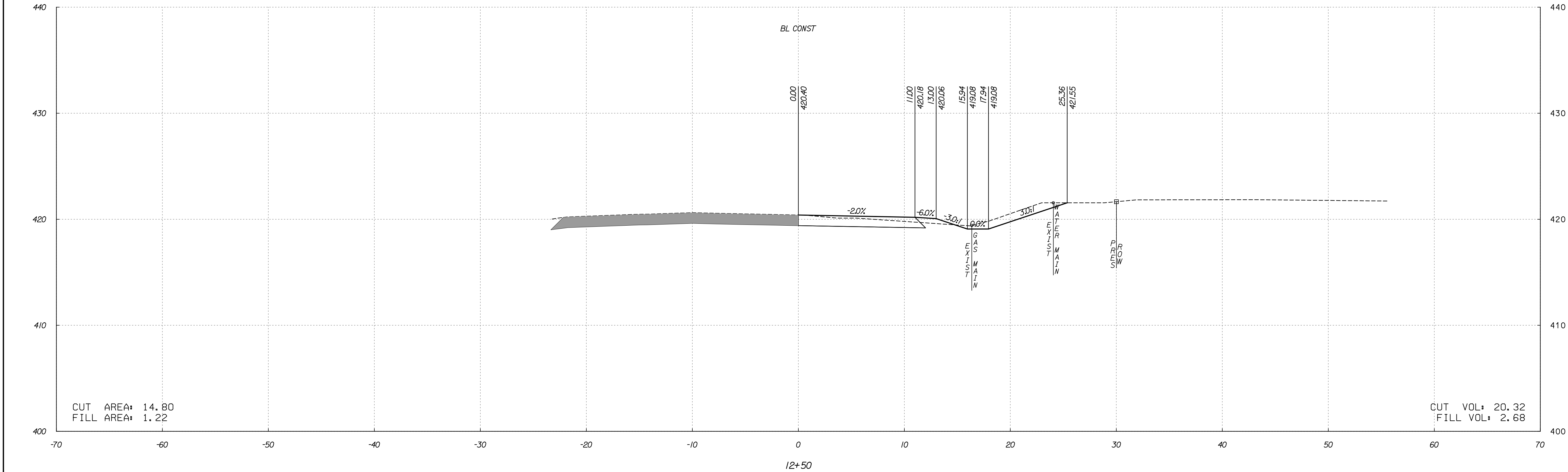


|  |                                      |                                    |                             |                |  |                             |               |
|--|--------------------------------------|------------------------------------|-----------------------------|----------------|--|-----------------------------|---------------|
|  | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  | SHEET TITLE                 | ROUTE         |
|  | DATE:                                | DATE:                              | DATE:                       | PERMIT         |  | ROADWAY CROSS SECTION SHEET | CR-17 @ CR-44 |



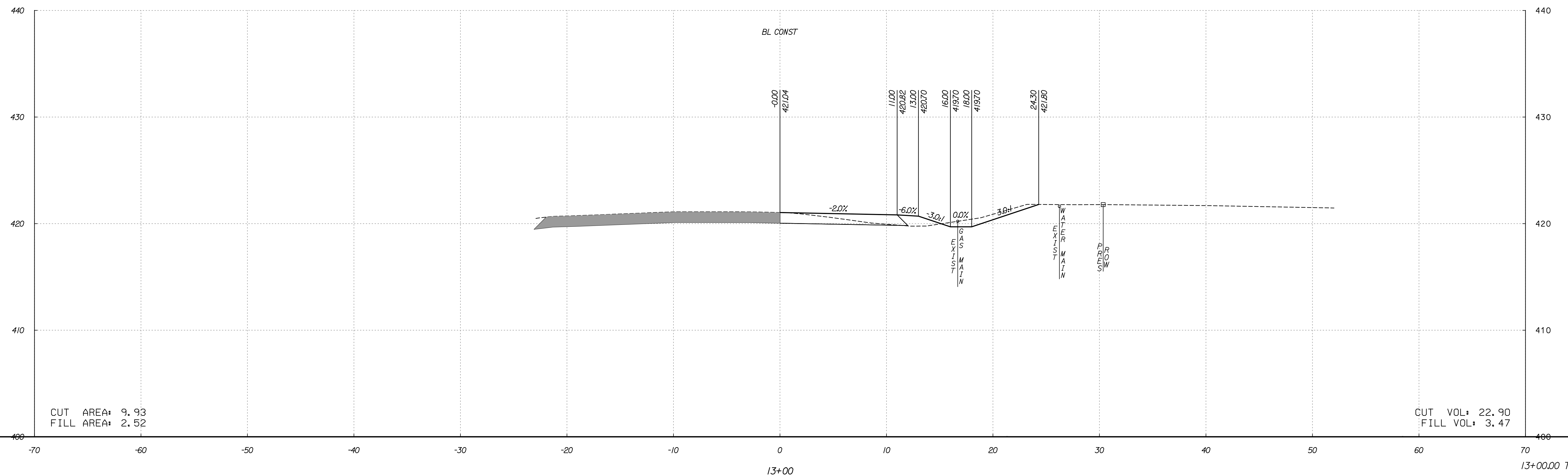
ROADWAY CROSS SECTION SHEET

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 51          |



|                                      |                                    |                             |                |                                       |                             |               |
|--------------------------------------|------------------------------------|-----------------------------|----------------|---------------------------------------|-----------------------------|---------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | HORIZ 0 5<br>SCALE (FEET)<br>VERT 0 5 | SHEET TITLE                 | ROUTE         |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         |                                       | ROADWAY CROSS SECTION SHEET | CR-17 @ CR-44 |

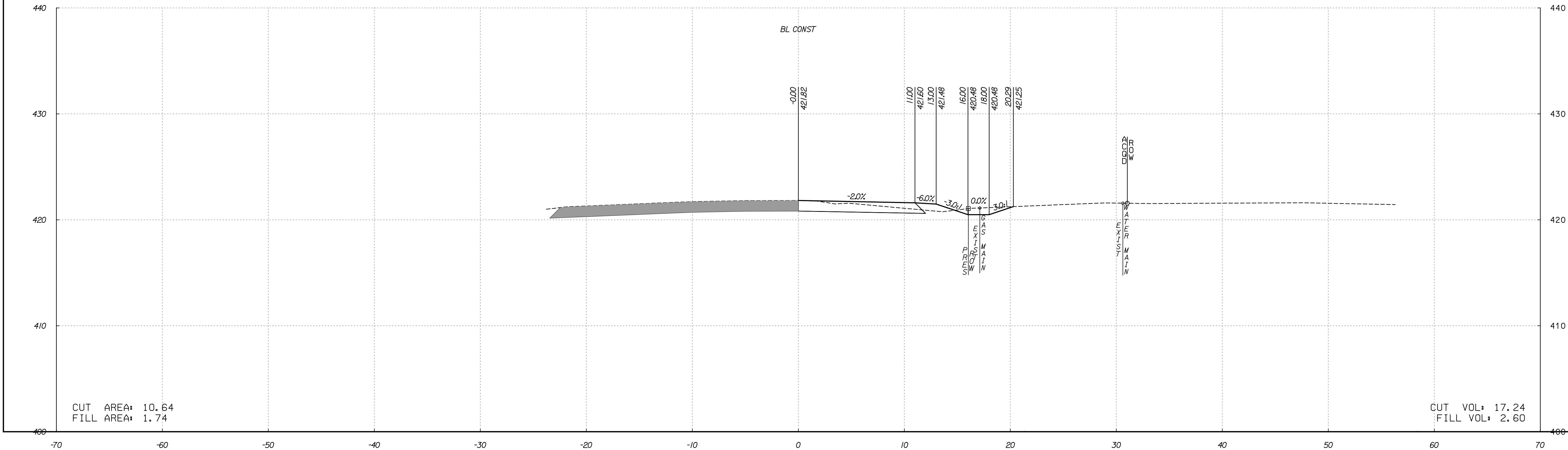
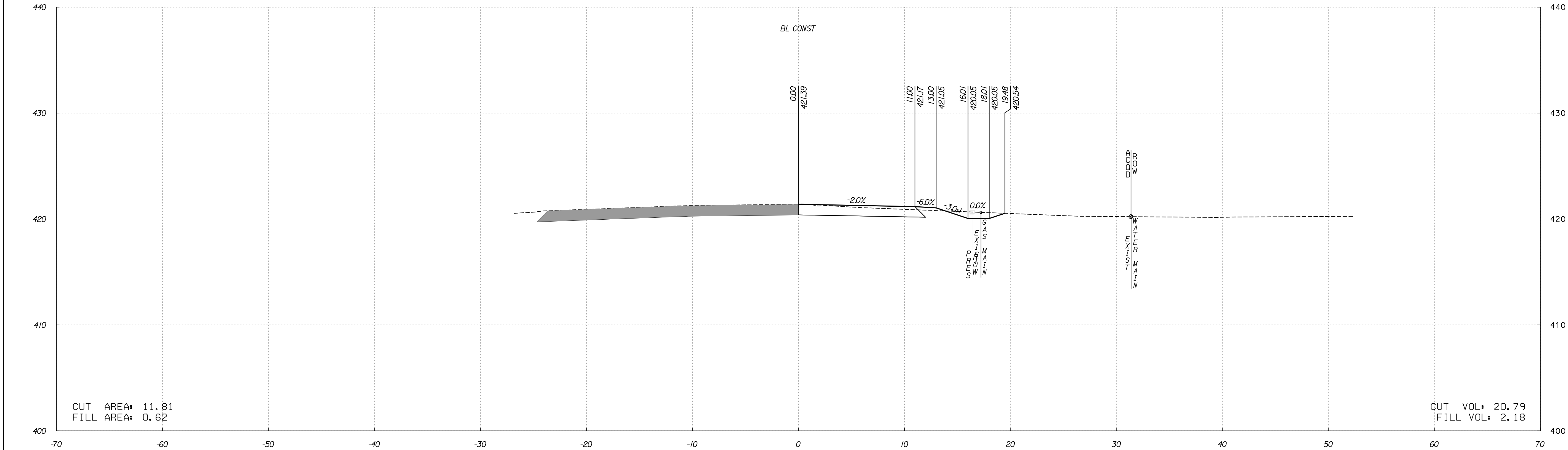
| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 52          |



|                                      |                                    |                             |                |  |                             |               |
|--------------------------------------|------------------------------------|-----------------------------|----------------|--|-----------------------------|---------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  | SHEET TITLE                 | ROUTE         |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         |  | ROADWAY CROSS SECTION SHEET | CR-17 @ CR-44 |

ROADWAY CROSS SECTION SHEET

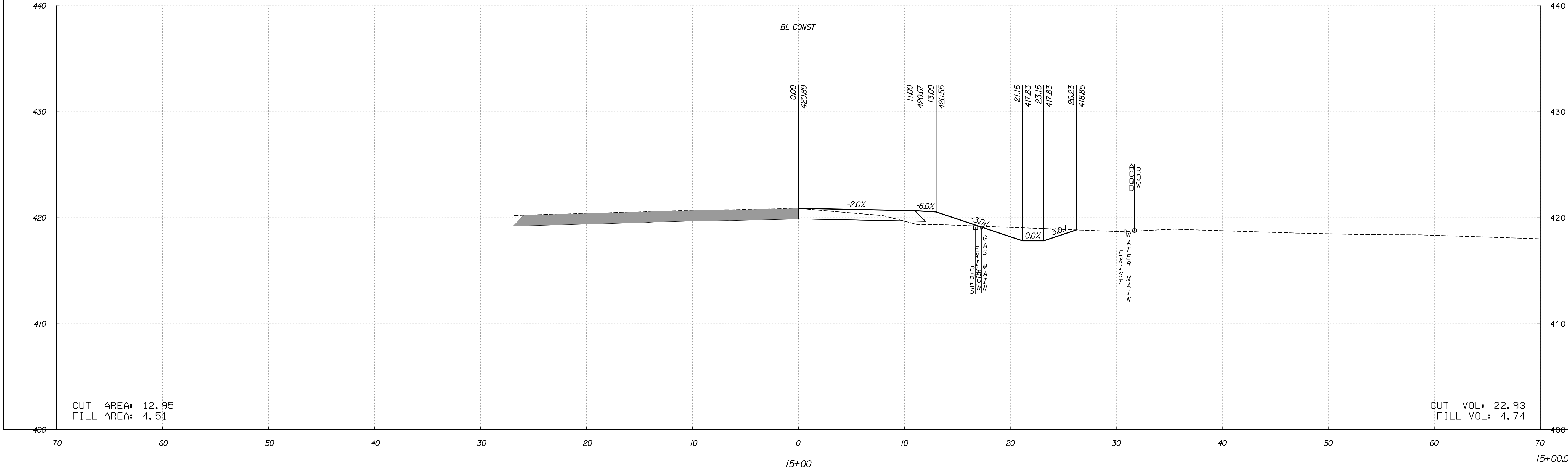
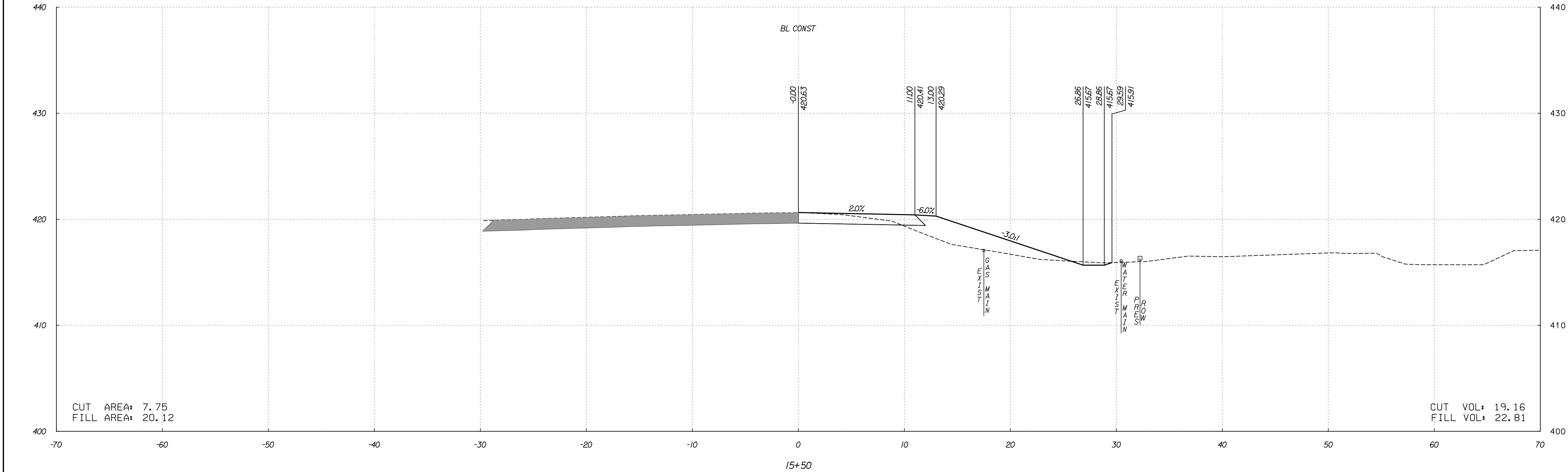
|                         |                |             |
|-------------------------|----------------|-------------|
| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
| SCP 59-889-18           | 2023           | 53          |



|                                      |                                    |                             |                |                                       |                                |                  |
|--------------------------------------|------------------------------------|-----------------------------|----------------|---------------------------------------|--------------------------------|------------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | HORIZ 0 5<br>SCALE (FEET)<br>VERT 0 5 | SHEET TITLE                    | ROUTE            |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         |                                       | ROADWAY CROSS<br>SECTION SHEET | CR-17 @<br>CR-44 |

ROADWAY CROSS SECTION SHEET

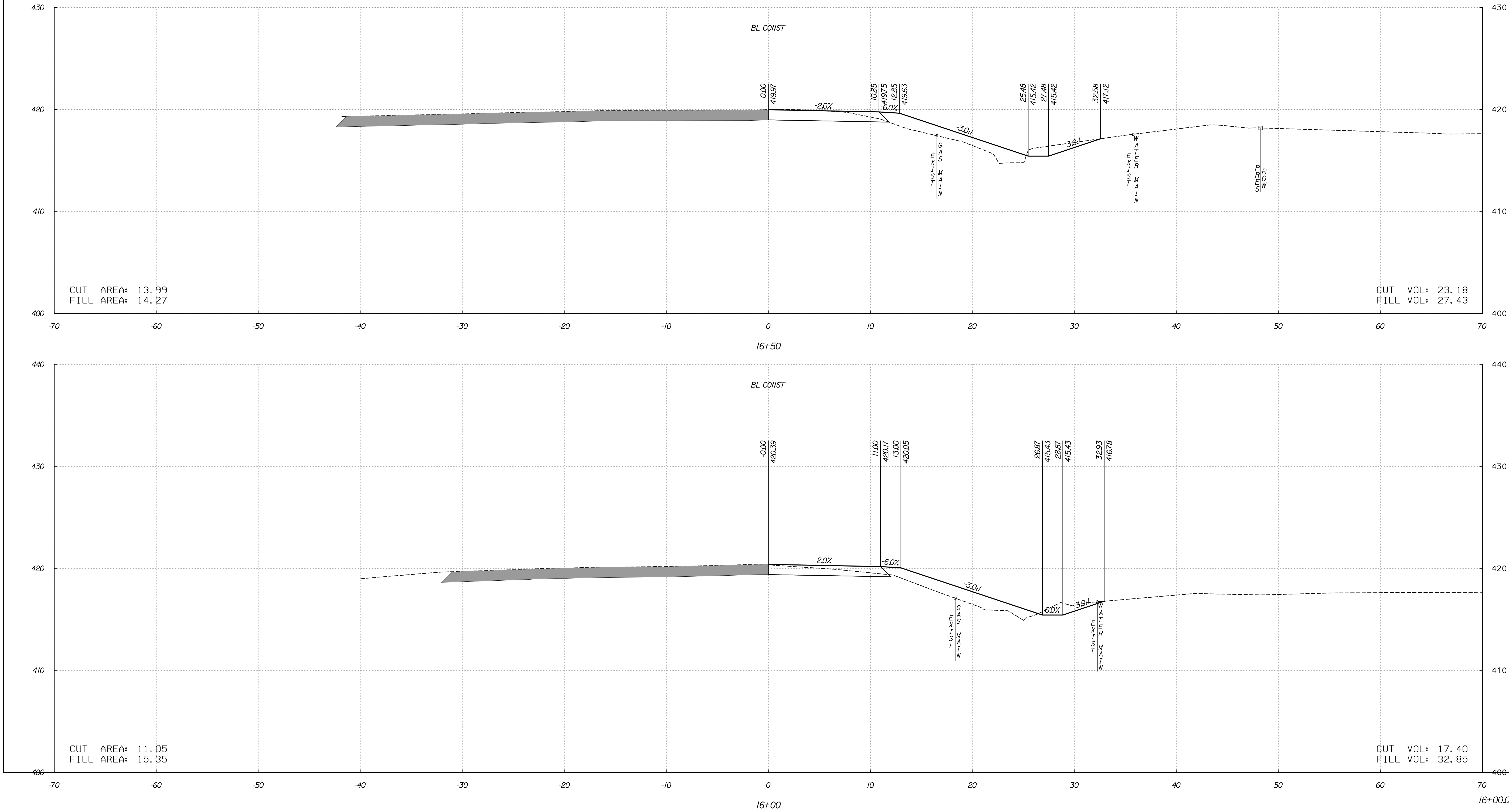
| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 54          |



|                                      |                                    |                             |                |              |          |                             |               |
|--------------------------------------|------------------------------------|-----------------------------|----------------|--------------|----------|-----------------------------|---------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL | HORIZ 0 5    | VERT 0 5 | SHEET TITLE                 | ROUTE         |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         | SCALE (FEET) |          | ROADWAY CROSS SECTION SHEET | CR-17 @ CR-44 |

ROADWAY CROSS SECTION SHEET

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 55          |

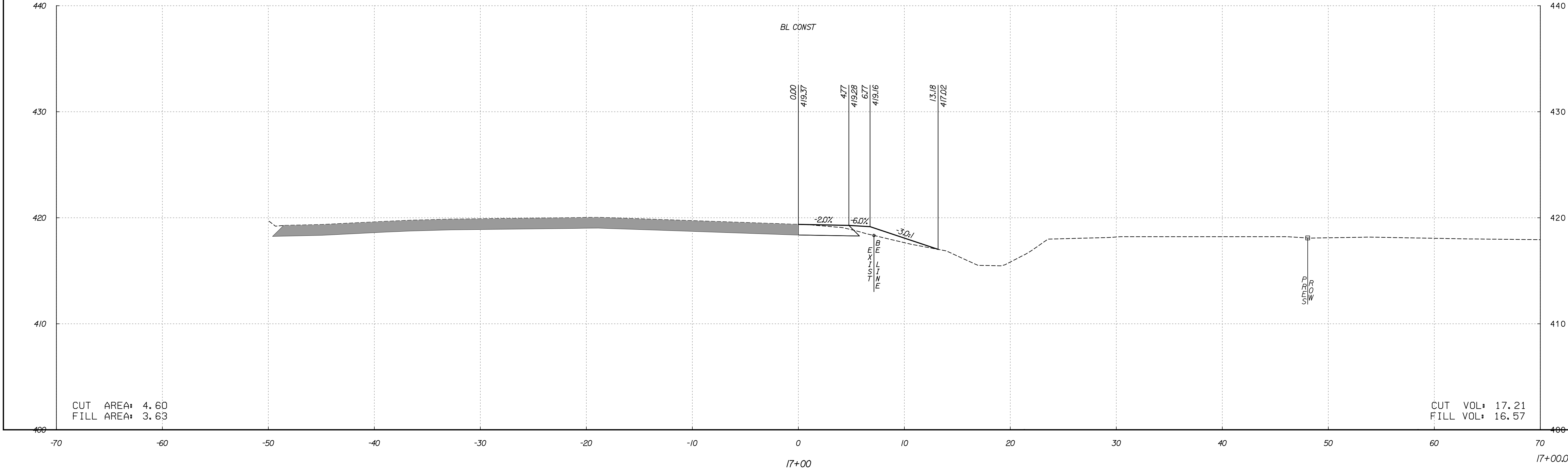
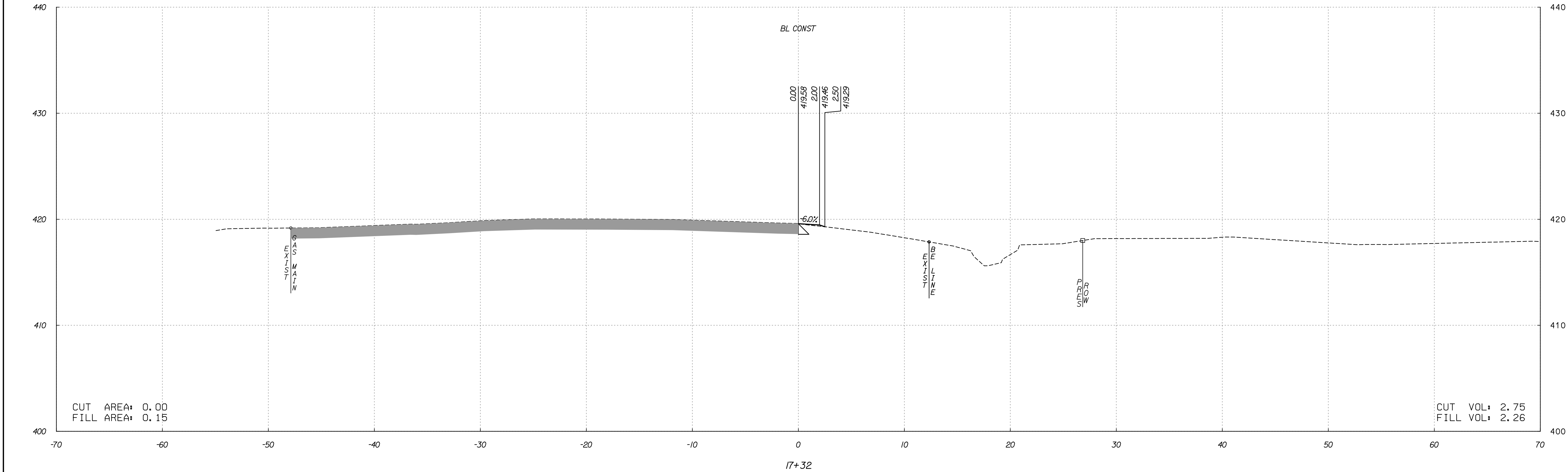


|                                       |                                     |                              |                |                                       |                             |               |
|---------------------------------------|-------------------------------------|------------------------------|----------------|---------------------------------------|-----------------------------|---------------|
| RESPONSIBLE PE: James R. Brown, P. E. | SUPERVISOR: Doug R. Peterson, P. E. | DESIGNER: Dayla Baugh, P. E. | PLAN SUBMITTAL | HORIZ 0 5<br>SCALE (FEET)<br>VERT 0 5 | SHEET TITLE                 | ROUTE         |
| DATE:                                 | DATE:                               | DATE:                        | PERMIT         |                                       | ROADWAY CROSS SECTION SHEET | CR-17 @ CR-44 |



ROADWAY CROSS SECTION SHEET

| REFERENCE<br>PROJECT NO | FISCAL<br>YEAR | SHEET<br>NO |
|-------------------------|----------------|-------------|
| SCP 59-889-18           | 2023           | 56          |



|                                      |                                    |                             |                |  |  |                                |                  |
|--------------------------------------|------------------------------------|-----------------------------|----------------|--|--|--------------------------------|------------------|
| RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  |  | SHEET TITLE                    | ROUTE            |
| DATE:                                | DATE:                              | DATE:                       | PERMIT         |  |  | ROADWAY CROSS<br>SECTION SHEET | CR-17 @<br>CR-44 |

| EARTHWORK SUMMARY SHEET   |                                      |                                    |                             |                |  |              | REFERENCE<br>PROJECT NO | FISCAL<br>YEAR   | SHEET<br>NO                             |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
|---|--------------------------------------|------------------------------------|-----------------------------|----------------|--|--------------|-------------------------|------------------|---|----------------------|---|---------|-------|------|-------|-----|-----|-------|-------------------------------|--|---------|-----|-----------|-------------------------|--|--|---|-----------|------------------|-------------|---|---|-------|--|-------------------|---|---|-------|-----------------------|--|--|---|---------|--------------------------|----------------|---|---|-------|--|----------------------|---|---|-------|-------------------------------|--|--|---|---------|------------------|--|--|---|-----------|
|   |                                      |                                    |                             |                |  |              | SCP 59-889-18           | 2023             | 60                                      |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| GN-2 NOTES: 148   |                                      |                                    |                             |                |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| <table><tr><th colspan="3">EARTHWORK SUMMARY (FROM CROSS SECTIONS)</th></tr><tr><th>ROADWAY</th><th>CUT</th><th>FILL</th></tr><tr><td>CR-17</td><td>213</td><td>124</td></tr><tr><td></td><td></td><td></td></tr><tr><td>TOTALS:</td><td>213</td><td>124</td></tr></table>  |                                      |                                    |                             |                |  |              |                         |                  | EARTHWORK SUMMARY (FROM CROSS SECTIONS) |                      |   | ROADWAY | CUT   | FILL | CR-17 | 213 | 124 |       |                               |  | TOTALS: | 213 | 124       |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| EARTHWORK SUMMARY (FROM CROSS SECTIONS)   |                                      |                                    |                             |                |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| ROADWAY   | CUT                                  | FILL                               |                             |                |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| CR-17   | 213                                  | 124                                |                             |                |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
|   |                                      |                                    |                             |                |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| TOTALS:   | 213                                  | 124                                |                             |                |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| <table><tr><td>UNCLASSIFIED EXCAVATION:</td><td>TOPSOIL BENEATH FILL</td><td>=</td><td>0</td><td>CU YD</td></tr><tr><td></td><td>CUT</td><td>=</td><td>213</td><td>CU YD</td></tr><tr><td colspan="2">TOTAL UNCLASSIFIED EXCAVATION</td><td></td><td>=</td><td>213 CU YD</td></tr><tr><td colspan="2">TOTAL BORROW EXCAVATION</td><td></td><td>=</td><td>162 CU YD</td></tr><tr><td>MUCK EXCAVATION:</td><td>MUCK ON CUT</td><td>=</td><td>0</td><td>CU YD</td></tr><tr><td></td><td>MUCK BENEATH FILL</td><td>=</td><td>0</td><td>CU YD</td></tr><tr><td colspan="2">TOTAL MUCK EXCAVATION</td><td></td><td>=</td><td>0 CU YD</td></tr><tr><td>TOPSOIL FROM STOCKPILES:</td><td>TOPSOIL ON CUT</td><td>=</td><td>0</td><td>CU YD</td></tr><tr><td></td><td>TOPSOIL BENEATH FILL</td><td>=</td><td>0</td><td>CU YD</td></tr><tr><td colspan="2">TOTAL TOPSOIL FROM STOCKPILES</td><td></td><td>=</td><td>0 CU YD</td></tr><tr><td colspan="2">REQUIRED TOPSOIL</td><td></td><td>=</td><td>133 CU YD</td></tr></table> |                                      |                                    |                             |                |  |              |                         |                  | UNCLASSIFIED EXCAVATION:                | TOPSOIL BENEATH FILL | = | 0       | CU YD |      | CUT   | =   | 213 | CU YD | TOTAL UNCLASSIFIED EXCAVATION |  |         | =   | 213 CU YD | TOTAL BORROW EXCAVATION |  |  | = | 162 CU YD | MUCK EXCAVATION: | MUCK ON CUT | = | 0 | CU YD |  | MUCK BENEATH FILL | = | 0 | CU YD | TOTAL MUCK EXCAVATION |  |  | = | 0 CU YD | TOPSOIL FROM STOCKPILES: | TOPSOIL ON CUT | = | 0 | CU YD |  | TOPSOIL BENEATH FILL | = | 0 | CU YD | TOTAL TOPSOIL FROM STOCKPILES |  |  | = | 0 CU YD | REQUIRED TOPSOIL |  |  | = | 133 CU YD |
| UNCLASSIFIED EXCAVATION:  | TOPSOIL BENEATH FILL                 | =                                  | 0                           | CU YD          |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
|   | CUT                                  | =                                  | 213                         | CU YD          |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| TOTAL UNCLASSIFIED EXCAVATION   |                                      |                                    | =                           | 213 CU YD      |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| TOTAL BORROW EXCAVATION   |                                      |                                    | =                           | 162 CU YD      |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| MUCK EXCAVATION:  | MUCK ON CUT                          | =                                  | 0                           | CU YD          |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
|   | MUCK BENEATH FILL                    | =                                  | 0                           | CU YD          |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| TOTAL MUCK EXCAVATION   |                                      |                                    | =                           | 0 CU YD        |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| TOPSOIL FROM STOCKPILES:  | TOPSOIL ON CUT                       | =                                  | 0                           | CU YD          |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
|   | TOPSOIL BENEATH FILL                 | =                                  | 0                           | CU YD          |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| TOTAL TOPSOIL FROM STOCKPILES   |                                      |                                    | =                           | 0 CU YD        |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
| REQUIRED TOPSOIL  |                                      |                                    | =                           | 133 CU YD      |  |              |                         |                  |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
|   | RESPONSIBLE PE: James R. Brown, P.E. | SUPERVISOR: Doug R. Peterson, P.E. | DESIGNER: Dayla Baugh, P.E. | PLAN SUBMITTAL |  | NOT TO SCALE | SHEET TITLE             | ROUTE            |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |
|   | DATE:                                | DATE:                              | DATE:                       | PERMIT         |  |              | EARTHWORK SUMMARY SHEET | CR-17 @<br>CR-44 |   |                      |   |         |       |      |       |     |     |       |                               |  |         |     |           |                         |  |  |   |           |                  |             |   |   |       |  |                   |   |   |       |                       |  |  |   |         |                          |                |   |   |       |  |                      |   |   |       |                               |  |  |   |         |                  |  |  |   |           |